

# Edmonton Bulletin.

## TO THE YUKON. THE ALL-CANADIAN ROUTE.

As time goes on the Yukon excitement increases, and everything points to a rush during the coming spring, the like of which the world has never seen. All classes of people in all countries of the world are affected by the craze. The Yukon rush differs from the California, the British Columbia and the Australian excitements in that, while they only attracted labor the Yukon is attracting capital as well. Besides, means of communication have been improved throughout the world since the last great rush, so that news travels more quickly and people can reach approximately the scene of the excitement more easily than ever before. The congestion of traffic last season was what caused the utter break down of the means of transportation on the routes of travel which hitherto had been found suitable and sufficient for all requirements. That the same routes and means will be 10 times more overtaxed during the coming season is already self-evident, and, therefore, unless vast improvements are made in them or unless new avenues are opened up or availed of there will be a congestion which will practically block all travel. Although there are many schemes talked of for improving the passes, constructing alternative railway routes, or increasing the steamboat transportation on the Yukon, none of these can in the nature of things become accomplished facts in time to meet the rush of early spring. Everyone is trying to get in first. Therefore the great rush of the coming season will find things practically as they were last fall on all routes to the Yukon. The prospector, therefore, has to consider which route as it exists to-day offers him the best facilities for reaching his destination. There is no necessity for haste in reaching this decision. There is at least a month which may be employed in seeking the fullest information on the subject. The wild scramble of last fall in which men struck out blindly they knew not where and cared not how, in the hope of getting through, need not be repeated. There is a great deal more information now available as to the geology of the country, the locations of the actual workings and the value of those workings, as well as regarding the practicability and cost of the several routes, than was last fall. So that the prospector is in a position to sit down calmly and consider what part of the Yukon he especially desires to reach, and then what is the best route to take in order to reach the location of his choice. The known gold bearing area of the district is about 500 miles from southeast to northwest by 100 to 150 miles in width. But this is the only part which has been prospected and it is more than likely that the much greater area remaining unprospected is also richly gold bearing. As the only means of transportation in the Yukon district is by boat or pack horse it is important to the prospector that he should strike the Yukon as near as possible to the district which he desires to prospect, as 300 or 400 miles travelling by boat or pack horse in the Yukon is a much more serious affair than the same distance by Pullman or steamer. As to the Klondike district, although that is the centre of the excitement, it goes without saying that all the good ground in that district was staked last summer. That point is only valuable to the new prospector now as an indication of what may be found elsewhere and of the conditions under which it may be found. When gold is found on any stream the tendency is to follow the stream to its source in order to find the richer deposits of the gold. Dr. Dawson sets this out very clearly in the report which he based on his exploration of the district in 1887. At that time only the bars on the large rivers were being worked. He advised the examination of the small creeks and gulches, and it was there that the Klondike finds were made.

### PELLY RIVER.

The Pelly is the main stream of the Yukon and has long been known to be richly gold bearing; but the best gold bearing portions being a long distance up stream from the coast or other routes it has been very little worked. The Macmillan, a large northerly branch of the Pelly, is now known to be rich, as a few weeks ago news reached the outside world of a strike on it equal to the Klondike. In view of the fact that the Klondike diggings have all been staked, it will be necessary for the men going into the Yukon now to look up other ground, and there is no doubt that the upper waters of the Pelly and Macmillan offer the most promising field to the prospector. The distance from Edmonton to Pelly river overland is roughly 1,000 miles. Taking the other routes, this point is about 450 miles up stream from Dawson and 250 miles up stream from the site of Ft. Selkirk, where all the coast routes which converge on the Lewes river meet the Pelly. When these distances of up stream navigation from the coast routes to reach the Upper Pelly are taken into consideration they do not compete with the Edmonton route. The man who is going to the Upper Pelly is simply going away from where he wants to get by taking the coast routes. But the miner going to Dawson is as near that place and can reach it more easily from the point where the Edmonton route strikes the Pelly than he can from lake Lindeman or Teslin lake. Another consideration in favor of starting from Edmonton is that a man purchasing his own horses here will have the use of them after he gets to the Yukon to move about with and prospect, crossing if need be from the Pelly to the head waters of the Macmillan, the Stewart or the Peel; while the prospector who goes in from the coast having only a boat must stick to the streams, and, therefore, is not in a position to prospect as advantageously as the man with an outfit of pack horses.

### SCIENTIFIC ASSURANCES.

Prof. G. M. Dawson, director of the Dominion Geological survey (in whose honor Dawson City is named) says in the official report of his examination of the Yukon country in 1887:

"Gold has also been found for a long distance up the Big Salmon river and on the Upper Pelly as far as it has been prospected. The Teslin-too, Big Salmon and Pelly have each already afforded some good paying ground." "Mining can scarcely be said to have begun in that region more than five years ago, and the extent of country over which the gold has been found in greater or less quantity is already very great. Most of the prospecting has been confined to the banks and bars of the larger rivers, and it is only when their innumerable tributary streams begin to be closely searched that 'gulch diggings' like those of Dease, McDame and other streams in the Cassiar district, and possibly even on a par with Williams and Lightning creeks in Cariboo, will be found and worked."

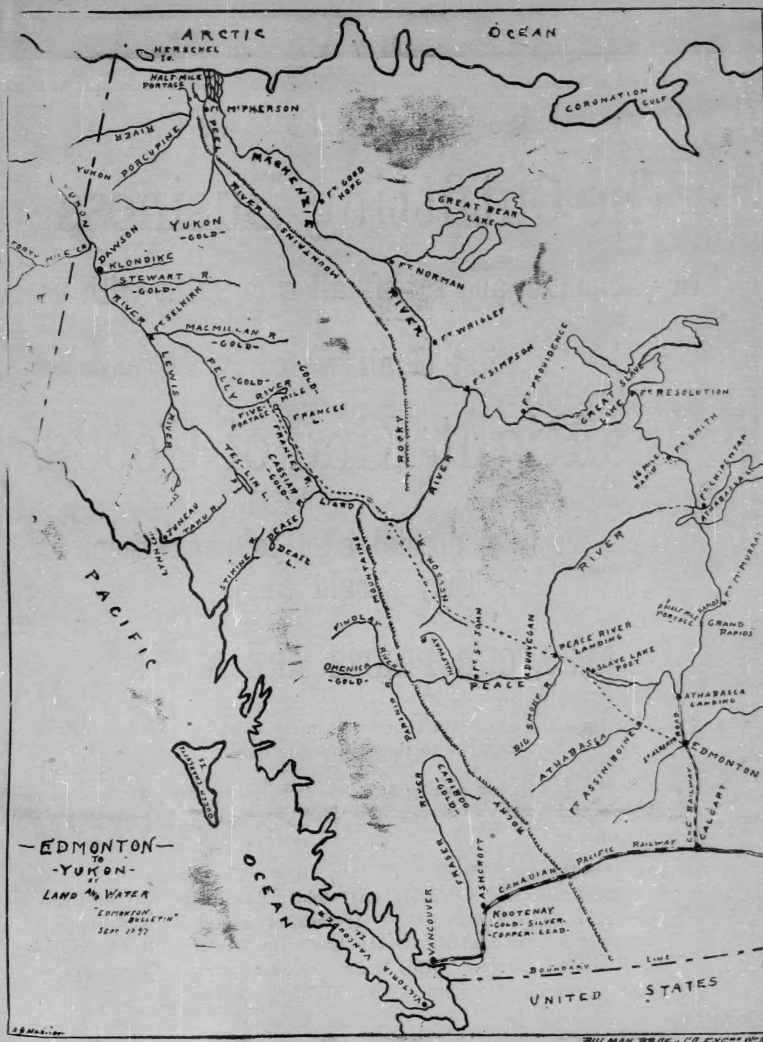
This prophecy, made 10 years ago, at a time when no one but the man who made it thought its realization possible, has been more than fulfilled in the case of Klondike, and gives weight to all the remarks made by Prof. Dawson (who is acknowledged to be the leading scientist of Canada, if not of America), on the Yukon district. He continues:

"The general result so far has been to prove that six large and long rivers, the Lewes, Teslin-too, Big Salmon, Pelly, Stewart and White, yield 'fine gold' along hundreds of miles of their lower courses. With the exception of the Lewes no part of the head waters of any of these have yet been prospected or even reached by the miners, and scarcely any of their innumerable tributaries have been examined. The developments made up to this time are sufficient to show that when means of access are improved, important bar mining will take place along all these main rivers, and there is every reason to anticipate that the result of the examination in detail of the smaller streams will be the discovery of much richer auriferous alluviums. Where these have been found and worked quartz mining will doubtless follow. The prospects for the utilization of this great mining field in the near future appears to me to be very promising."

If Prof. Dawson were using this language now it would be considered a bold attempt to create a boom; but when it is remembered that it was put in print 10 years ago as a result of a personal examination of the field by the Professor in his capacity as government geologist, and when very little exploration or work had been done, it has a value in view of the extraordinary manner in which it has been verified that must be admitted to be very great. It will be noted that the Professor lays special stress on the desirability of prospecting the smaller streams and the head waters of the rivers. As the Edmonton route leads most directly to the upper waters of the main stream of the Yukon, of whose likelihood to be richly gold bearing Prof. Dawson speaks so encouragingly, that is the route for the prospector looking for new fields to take. If he is not suited on the upper waters he can easily float down stream. But if disappointed on the lower waters it is a long and hard pull to get up stream to the head.

### ALL GOLD BEARING.

Not only does the Pelly offer the most likely field for the prospector at the present time of the whole Yukon region, but the Liard, up the valley or waters of which the Edmonton route goes, is also known to be gold bearing. Gold was struck on the bars of the Liard west of where it passes the Rockies in 1872 by Thibert and McCulloch. They went on up the Liard and Dease rivers and struck the Cassiar diggings on Dease river and lake. From '73 to '87 these diggings yielded \$5,000,000. The miners and their supplies came in from the coast. During the height of the excitement men went on to the Liard and up the Frances, and found paying diggings, but the cost of getting in supplies by that route to the Upper Liard and Frances river was so great that the miners withdrew, leaving the richness of the region demonstrated and yet only very little prospected. The subsequent construction of the railway to Edmonton has furnished a new base of supply and materially altered the conditions. The Cassiar range of mountains lying northwest and southeast parallel to the course of the Liard on its southwesterly side, is no doubt the source of the gold. This range crosses the Dease a short distance below and north of the richest diggings. Prof. Dawson reports that \$10 nuggets were taken out of Sayyee creek, a small tributary of the Liard, on the northeast



side of the range, by a party of miners who wintered there in 1875, but four of the miners died of scurvy and the creek was abandoned. Of the Liard, above the mouth of the Dease, Prof. Dawson says:

"The gravel bars and shores of this part of the Liard are almost half composed of rolled quartz pebbles. The quantity of such vein material present in this district may be regarded as a favorable indication in respect to mineral development. Some small bars have paid to work along this part of the river, and gold is also found in some layers of the gravel deposit which overlies the older rocks along the canon and above it, where wages at \$4 a day can be made. The amount of cover which it soon becomes necessary to remove in following the paying layers has prevented extensive mining, but probably these gravels might be advantageously worked as a whole by sluicing or by the hydraulic method."

Of Frances lake, Prof. Dawson says:

"There is, too, a notable abundance of quartz along all the beaches of the lake, this material being derived from innumerable veins which traverse the schists in all directions, though most often found parallel to the bedding planes, and generally assuming forms more or less lenticular. The largest of these are often several feet in width, and those seen in the canon of the Finlayson near its mouth, are of workable dimensions if only moderately rich in gold. Specimens of quartz veins, containing some iron and copper pyrites, from the east side of the east arm about midway up on it, were found to contain traces of gold on assay by Mr. Hoffman, the government analyst."

"In general appearance the rocks of Frances lake very closely resemble those from which the rich placer gold deposits of Dease lake are derived, and they are probably of about the same age. Several 'colors' to the pan were obtained from surface gravel at the mouth of Finlayson river, which struck me as especially promising in aspect, and there seems to be no reason why some of the streams flowing across the schistose rocks into the lake or in its vicinity should not prove richly auriferous. This entire district well deserves careful prospecting."

After having reached the above conclusions, Prof. Dawson was told by two miners whom he met that they had made \$8 to \$9 a day at the mouth of the Finlayson, which empties into Frances lake and forms the connecting route to the Pelly.

Black river, which comes into the Liard from the south, west of the Rocky mountains and below the mouth of the Dease, has, on the authority of Prof. Dawson, been mined to some extent, yielding from \$6 to \$20 a day per man.

The Nelson, which joins the Liard east of the Rockies, was prospected near its head this season by J. Langlais, of St. Albert, and indicated fair pay.

The bars of the Peace in the vicinity of Ft. St. John have been mined for many years. Over \$1,000,000 has been taken out of the Oménica diggings on the upper waters of the Peace, west of the mountains, and the region is still largely unprospected. Several wealthy companies are now putting in expensive sluicing works.

The Athabasca was mined extensively several years ago above and below where the Edmonton trail crosses, and the McLeod, which empties into the Athabasca from the southwest about 120 miles northwest of Edmonton, yields considerable gold every year.

The Saskatchewan, upon which Edmonton is situated, has been mined for many years and still yields from \$25,000 to \$50,000 per season in gold dust. Improved appliances are now being introduced and it is hoped this yield will be largely increased.

It is a peculiarity of the Edmonton route that from its commencement on the Saskatchewan it lies in gold bearing territory which increases in richness as the Yukon is approached. In such a distance there is a vast field for the profitable employment of labor and capital which is well worth careful investigation by the ordinary prospector as well as the trained expert.

(CONTINUED ON PAGE FOUR.)





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## WHAT OTHERS SAY REOUTFITTING.

Edmonton, Feb. 5, '98.

TO WHOM IT MAY CONCERN:

In the interests of any parties contemplating a trip to the northern gold fields by way of Edmonton, I wish to speak a word of advice as to where and how to outfit. Many parties are hurried over to the north, not advisable to wait until Edmonton is reached to purchase their outfits and they are told that the stocks which the merchants carry are too small to outfit parties properly. These latter statements I wish to most emphatically deny. The members of my party, numbering in all 16, have outfitted entirely here. We did not bring a dollars worth of goods with us and are glad of it as we can procure all we want here in unlimited quantities and at most reasonable prices and besides we have the satisfaction of knowing that we are getting goods suited to our purposes. I would most sincerely recommend any party to come through and outfit entirely here.

P. J. CURRAN.

LESSER SLAVE LAKE.

Jan. 17, '97.

MESSES. LARUE & PICARD.

GENTS: Your agent, Mr. Beauchamp, at this place has given us the freedom of the camp and extended to us all the courtesies of the frontier and, generally, has treated us royally, all of which is, and shall always be, highly appreciated. His prices for everything here we found so reasonable and the stock so complete that we regretted having so much stuff in Edmonton. Beef, pork, lard, butter and hay almost at prices of same in your town, and the kindness of both him and the entire people, Indians included, has been one of the surprises to the entire party. All the talk of the dishonesty of the natives we found a base falsehood, on the contrary they are hospitable, honest and seem quiet, thrifty, dressing better than any of our United States Indians. As to the trail via Assiniboine and across there is now a good feasible route for flat sleds the way we came, of course we being the first party to use it, had some difficulty in getting through, but not nearly as much as we anticipated, and it was an agreeable surprise, good feed and the weather so pleasant, that for us who were used to roughing it it was really a pleasure trip. The worst and steepest hill on the entire trip was not one-half as high as the hill at Edmonton from the river to town, and as to the feasibility of a wagon road it is simply a boy's play for your people to put it through. Thanking you again in the name of the party.

I am yours  
Geo. W. FUGARD.

## "THE MANLY KLONDIKER."

The following is a condensed report of a sermon preached by special request in the Edmonton Methodist church, on evening, Jan. 30th, to a crowded audience of Klondikers and citizens. The Rev. Mr. Dyke based his sermon on 1 Cor. XVI: 13-14. "Stand fast in the faith. Quit you like men. Be strong. Let all your things be done in charity."

The preacher said there is no other attraction under heaven which fascinates humanity like the prospect of immediate wealth. This great gold excitement is not provincial, national, or even continental. It is world wide. Everybody is so deeply interested that the chief topic of conversation is Klondike routes, clothing and mines. In the opinion of the preacher the pulpit should not neglect these great movements of society. The sacred desk should be something more than a finger post, a guide, an experienced companion, showing how to get through this world in the safest and best manner. The books of Providence and Revelation teach us that there is a divine plan in which the best things of this world are intended for the best people. Not necessarily those of hereditary title, for a wise and distinguished man may be the father of a consummate fool. Nor what some people call "blue blood," though it is a distinct advantage to be well born; but I do mean that the best things in this world are intended for the manly man and the womanly woman. God Almighty who placed the gold in the mountains, through the valley and along the rivers desires that its attainment shall elevate and not degrade humanity. It is the divine wish that the gold be obtained by men who while they recognize God's goodness will also remember the practical value and distinguished honor of manifesting a noble character. The Klondiker will get lots of gratuitous advice, but whatever others may say I have great pleasure in repeating the words of the Lord, God Almighty "Watch ye. Stand fast in the faith."

In going to the Klondike let me beg of you not to throw away your religion, "Stand fast in the faith." If you have been brought up in Church of England beware and take the prayer book along. Seriously read some collect, scripture and prayer every day, and as you read take pains to "Mark, learn and inwardly digest the holy truth." If you have been brought up a Presbyterian, do not cease singing psalms and reciting the shorter catechism. It may not be necessary to keep up a continual argument about election, and predestination. It were infinitely better for every man to show by his life that he belongs to the elect of saving grace. If you have been brought up a Baptist do not exhaust yourself and others arguing that baptism by immersion is the only scriptural mode. There will be plenty of opportunity for every man to show that he has not only been baptized with water, but by the Holy Spirit of God. If you have been brought up a Methodist do not be ashamed of your church. Once in a while in camp and out of camp sing with all your heart some of the best known Methodist hymns. Hold an experience meeting. Organize a Sunday school. Endeavor with all your might to make the best and highest "strike," and then with all your soul try to be the fairest and most generous with what you find. If you have been brought up a Roman Catholic there will be many times when your good priest is too far away to give his kind offices. You will often be in circumstances where the sacred emblem of the cross and the image of the Holy Virgin will not be before your gaze to remind you of holy things. Just then try to remember and practice the good lessons you have been taught. The people who rush to the gold fields will need the best things in all the churches to keep them in the sacred walks of righteousness. If there is a man here who has not been brought up to anything, or who has deserted the Christian faith, this had better get aboard some section of the gospel train right now. If you come in the right spirit any of the churches will welcome you. The Methodist door is wide open. If you are a Jew, you can give you a first class ticket. The steward will look after your fare, and on good behavior the conductor will guarantee you a safe passage to a better life. Accept the faith of the Lord Jesus. Stand fast therein for gold is better with wisdom and Christ, than rubies and diamonds in a Christian's life. Stand fast in the faith of Christ.

In going to the Klondike I beg of you not to throw away your common sense, "Quit you like men. Be Strong." Among

the elements of common sense I exhort you to be prudent. Take good care of your health. There is no more convenient place than the bath shops and laundries. Every cook who goes north may not have a diploma for skill in the culinary art. The banquets may sometimes be burned, the mat fall into the ashes, the coffee pot be accidentally kicked over, the salt get lost on the march and the sugar get soaked in the rain. You may at times be worried with flies, find yourselves sleeping out of doors in the chilling rain or biting frost. But what ever happens take care of your health. All the wealth of the Yukon will not compensate a man for the loss of his health. Hercules would amount to nothing if he had to row along life's stream in a rotten boat. Be prudent. Take sufficient rest. Do not travel on Sunday. Exercise care in the matter of food and cleanliness, then you may return not only rich in pocket, but with robust health. Be prudent in your outfit and companions. Take plenty of the right kind of food, clothing and tools. Careful though you may be you may much chagrin when you are a thousand miles from Edmonton. Leave the cranks at home. Cheerful good nature, the pleasant laugh and joke, the true hearted and unselfish are the best companions. You will manifest common sense by being strong in courage and industry. The long march will test your courage. Some may grow faint hearted and turn back. But as for you, brave out every obstacle. Fire out every difficulty. Get there? Then be industrious. You may have to dig more than one hole before you find a creek lined with nuggets. Remember you are not going to grapple with the stern hardships of the Arctic zone for the fun of the thing. You are going to work for yourself, your loved ones and your country's good. Others are watching you with earnest gaze. If you can find gold in paying quantities on the Peace or Pelly, the Nelson and Liard rivers, whatever may be said of other routes, this one will command the shortest march and the biggest procession of gold seekers the world has ever known. But whatever you do—be men. Aim at success. Tell us the actual truth. You have heard how Mark Twain has said that a "gold mine is a hole in the ground and a bigger liar on the top." Tell us the actual truth and if you succeed this town will soon become a city and Alberta, Assiniboine and Manitoba will throbb every pulse with the hum of growing industry.

My concluding wish is that in going to the Klondike you let all your things be done in charity. Be considerate of one another. Some parties may have accidents, sickness or unfortunate occurrences. Be considerate. Show all the kindness you possibly can. There will be thousands of opportunities for manliness. Let each vie with the other in constant kindness and consideration. Let your charity not only express itself in the far north, but when you have made your pile, remember the benevolent institutions at home. Think of the poor and unfortunate. Have a noble purpose in getting wealth. Without such a purpose wealth degrades manhood. A cynical fellow who had sacrificed manhood to business has the following epitaph placed upon his tomb stone. "Born a man, died a grocer." See that it may never be truly said of any of you "Born a man, died a Klondiker." Whether you get riches in the far north, or not, live for the good of humanity. Live to the ages to come shall bless God for your existence.

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Flour, Feed and Groceries, Edmonton.

## NOTICE

Is hereby given that application will be made to the Parliament of Canada at the next session thereof for an Act to incorporate a company to construct, maintain and operate a railway or tramway from the town of Melfort on the foot of the White Horse River, on the Lanes River, in the Northwest Territory. Also to acquire lands, houses or other aid from the Government of Canada.

GEO. E. KIDD, Solicitor for Applicants.

Ottawa, Ont., Dec. 9, 1897.



## FOR SALE!

That desirable dwelling house situated on College Avenue, Edmonton, immediately east of the public school, known as the Ibbotson house. Fine long lot. As the railroad is sure to pass through Edmonton next summer, property will double in value before spring. Buy now before prices advance.

For particulars apply to

R. RITCHIE,  
South Edmonton.

## Pigs Wanted!

The Edmonton Pork Packing Co. are now buying live pigs delivered at their curing house (Donald Ross' old hotel). The highest market price paid.

W. S. EDMISTON,  
Manager.

## NOTICE

It is hereby given that application will be made to the Parliament of Canada at its next session by The Central Canada Loan and Savings Company of Ontario for an Act to enable the said company to carry on business anywhere in the Dominion of Canada, to consolidate, define and declare its liabilities, obligations and powers, and to carry out the foregoing by incorporating its shareholders as a new company.

Dated at Toronto, Dec. 1st, 1897.  
K. T. MALONE,  
Solicitor for Applicants.

## TOWN LOTS

In the vicinity of Stations on the Calgary & Edmonton Railway

## FOR SALE...

Apply to C. S. LOTT, Calgary.  
H. WILSON, South Edmonton, or  
ORLER, HAMMOND & NANTON, Winnipeg

## HOTELS

**ALBERTA HOTEL, Edmonton.**—The above well known hotel having been recently enlarged and improved, now under the management of Jackson & Grierson, is prepared to offer the best accommodation to visitors and the public generally. Table unexcelled. Good sample rooms. Livestock in connection. Bus meets all trains.  
JACKSON & GRIERSON, Props.

**JASPER HOUSE, north side of Main street.** The only hotel in Edmonton. The largest house in Northern Alberta. First-class weekly and daily board at reasonable rates. Commensurate sample rooms in connection. Also first-class livery and feed stable.  
J. GOODRIDGE, Proprietor.

**QUEEN'S HOTEL, Edmonton.** Strictly first class in every particular. Free bus meets all trains. Free sample rooms.  
NEVILLE WHITE, Proprietor

## Apples

Just arrived at Lauder's Bakery a heated car of Winter Apples—Spys, Baldwins, Greenings and Kings. We will put a limited quantity of the above by the barrel at \$7.00 per barrel delivered.

LAUDER'S BAKERY.

## Sturgeon Coal

Quality unsurpassed, and the best on the market. Delivered in town \$2.50 per ton; at mine \$1.00. The Sturgeon Mines.

Cash on Delivery.

lapi FRANK SMITH, Manager.

## Telephone S. Moran FOR COAL

At \$2.00 per ton. Cash on delivery.  
S. MORAN, Edmonton.

## Fort Saskatchewan MILLING COMPANY.

BRANDS:  
"Alberta Patent"  
"Alberta Strong Bakers"  
"Our Daily Bread"  
"X X"

Wholesale Orders a Specialty. Traders would do well to enquire for prices before purchasing elsewhere. Once used always used. More bread to the sack than any other Flour manufactured in Alberta.

FORT SASKATCHEWAN AND STURGEON MILLS.

Post Office—Fort Saskatchewan.

Merchants and Traders supplied from the Wholesale Depot at Edmonton. All brands in stock.

W. S. ROBERTSON, Agent

NOTICE.

We understand that flour has been offered for sale in unbranded sacks, and represent as the product of our mill, and we wish to warn the public that we will be responsible for neither the quantity or quality of any flour claimed to be our product unless in sacks branded with our trade mark.

FORT SASKATCHEWAN MILLING CO.

LUMBER FOR SALE

AT STURGEON MILL

All kinds of lumber for sale. Best lumber a specialty. Call or write for prices.

Prudent People Protect their

Property by insuring in the

CANADIAN

FIRE INSURANCE COY.

W. J. RICHARDSON

LOCAL AGENT, EDMONTON.

## TELEGRAPHIC.

WINNIPEG, Feb. 10, '98.

Several lives were lost at a fire in Pittsburgh, Pa.

President Barrios, of Guatemala, was assassinated.

The trial of Emil Zola was the scene of renewed disorder.

Wonderfully rich discoveries gold are reported from Alaska.

The population of Greater New York is estimated at 3,458,890.

Lord Salisbury charges France with breach of good faith in Madagascar.

The Imperial parliament was opened by the Royal commission on Tuesday.

It is reported that the conservative party will oppose the Stickeen railway bargain.

Paul Kruger was re-elected president of the Transvaal republic by a large plurality.

Russia has abandoned the candidature of Prince George, of Greece, as governor of Crete.

The C. P. R. are engaging laborers for the Crown's Nest Pass railway in Newfoundland.

The United States government have ordered four companies of infantry to proceed to Alaska.

Mrs. Canova and her mother were committed for trial for murder at Woodstock, New Brunswick.

Lord Salisbury explained Great Britain's position on the Chinese question in the house of Lords.

A revolution has broken out in Nicaragua and United States has landed marines at San Juan del Sur.

In the United States senate three resolutions reflecting on support of recognizing Cuban belligerency.

Leutnant was found guilty of murdering his wife and was sentenced by the jury to imprisonment for life.

Canadian exports to Great Britain for January show considerable increase. Imports for 1897 decreased.

A deputation of the Dominion alliance waited upon Sir Wilfrid Laurier regarding the prohibition plebiscite.

The steamer Gallia due at Halifax on Monday last has not arrived. E. S. Pickett, of Edmonton, was on board.

Senor Dupuy de Lome, Spanish minister at Washington, is accused of writing a letter reflecting on President McKinley.

Blair, minister of railways, introduced a bill in the House to provide for the Stickeen-Teslin lake railway and explained the context.

## LOCAL.

A COMPANY has been formed to construct a canal between the Saskatchewan river and Lake Superior.

E. KING, of the Hudson's Bay Co., Calgary, came up last train to take a position in the company's store here.

F. C. STEVENS, of the Meriwether party, returned from the camp at Grand rapids on Monday and went south on Tuesday's train.

D. J. GOGGIN, superintendent of education for the Territories, arrived on last train to conduct the final examinations of the normalists here.

PEARCE BROS. are moving their portable saw mill from Egg lake, (north) to the Landing where they will saw lumber for McDougall & Secord.

The anthem at the Presbyterian church Sunday evening next will be "Hark, Hark, My Soul," by Shelley. R. McIntyre will render a sacred solo. "Saved by a Child."

CONST. KITCHEN arrived from Wetaakiwin last week with a prisoner named Strone, convicted of house breaking and sentenced to two months at Fort Saskatchewan.

A WEEKLY stage line from Edmonton to Athabasca Landing is being put on by local citizens. It is to be ready for business on March 1st and will carry freight and passengers.

K. A. McLEOD has gone into boat building for the Yukon trade. He has also purchased the engine, boiler, machinery and cars of the Chicago Steam Sleigh Co. The machinery may be used on a steam boat and the cars he intends to rent for residences.

The Prospecting and Mining Company, of Mount Pleasant, Iowa, have written to John Kelly, of the Carriage Works, stating that they intend sending a company of men to the Yukon and asking if he can supply them with sleds with axle and wheel attachment so the sleds can be changed into carts when the snow melts. Mr. Kelly is having a plan prepared of a sled such as they desire and will submit it to them.

The Chicago company who were constructing a steam sleigh here go overland to the Yukon, made a trial of their machine a few days ago when it was found to be impracticable and would not work. The members of the party had been to great expense in buying their machinery and engine as well as building their four cars, but were fortunately able to dispose of the complete outfit to K. A. McLeod for a fair price. The party intend continuing their journey to the gold fields. Some will go by the McKenzie route when spring opens and the remainder will probably go overland by pack horses.

The prize list for the carnival to be held on the Thistle rink here on the 16th is as follows: Ladies prettiest costume, a clock; best representation of a nursery rhyme combined gold chain and brooch; most comical dress, lady or gentleman, silver whistle marker. The winners are at liberty to exchange for others of equal value in E. Rayner's store. Girls under ten years of age, costume, plush case containing two bottles perfume; boys best dress, a Chinese silver pin; most graceful lady skater, elaborate solid silver ornament; best fancy skater, a Chinese silver pin. Prizes will be exhibited in E. Rayner's window. On account of selling tickets beforehand it has been found necessary to charge those in costume as well as spectators as the management are hoping neither expense or trouble to make the carnival a success.

The following telegram was received by the police here on Monday last from S. P. Collier, chief detective of the Chicago police department: "Arrest and hold Asa Hodgman, alias J. S. Baker, alias Johnstone with whom at Sheppard's, waterbury here for embezzlement." Further word was received from the authorities in Calgary that a warrant was sworn out for Johnstone's arrest and to arrest and hold him pending its receipt. Sgt. Irvine, therefore, went on Monday and arrested Johnstone at the South Edmonton Hotel, where he was staying, and brought him over to the barracks here. On the Chicago authorities being advised of Johnstone's arrest they wired instructions to hold him at any hazard and that a man was leaving Chicago immediately to take him in charge. Johnstone went south to Calgary on this morning's train in charge of Sgt. Anderson and extradition papers will be prepared there.

SAM. McNAMARA, of Calgary, came in on Monday.

Geo. S. WILKINS, Red Deer, came in last night.

Jas. H. ROSS, Commissioner of Public Works, Regina, arrived on last night's train.

MESSRS. Benj. R. Sears and J. McDonald, of the Fresno party, and H. Burbank, a trader from Fort St. John, arrived in town a few days ago.

INSPE. SNYDER was met by Messrs. Sears and Burbank at Montney river, half way between Devanag and St. John on the 8th of January.

A ROCKY match between the junior teams of the north and south sides, respectively, takes place on the rink here to-morrow evening.

MR. McKINLEY has purchased a saw mill and will saw lumber for farmers on Stony Plain for a month, and then will move north of his farm on the Sturgeon.

AN urgent meeting of the liberal association will be held in F. E. Time's office to-morrow evening at 8 o'clock, to elect officers and discuss important questions of state.

THE City Carriage Works have turned out this winter three hundred and twenty Klondike sleds for the Yukon trail. John Walters has also turned out an equally large number.

MESSRS. J. L. Robertson, A. Pugh, G. L. Linworth, P. Lawson and J. W. W. pulled out with flat sleighs for Peace river on Tuesday afternoon, by way of the new Slave lake road.

JAS. MARTIN, of Winnipeg, Territorial manager of the Mutual Reserve Fund Life Association, of New York, arrived in town on Wednesday's train and will have his headquarters here.

EWEN MACDONALD, factor of the Hudson's Bay Co., stationed at Lesser Slave lake, and J. Rankin, late of the Company's service at Mattawa, Ont., arrived on Monday.

MR. Rankin will take the position of accountant at the Lesser Slave lake post.

R. MOX, who with his companions Messrs. Falkenberg and Sejersted, of Chicago, was wintering at Grand rapids en route to the Yukon, returned to town on Monday. He will go back to Chicago to attend to some business matters and intends returning in the spring to continue his journey.

A branch of the Birkbeck Investment Security & Savings Co., of Toronto, has just opened in Edmonton under the charge of Mr. G. W. R. Almon. A very strong local board has been secured and business will also be done in North Edmonton. This company has a splendid record during the five years it has been in business in his resignation as principal of the public school here. The resignation takes effect on April 1st. Immediately upon giving up his position Mr. Mackenzie will open up a book and stationery business here with a large stock of goods which he has already ordered.

FREE PRESS: Mr. John Wilmott, who has been connected with the Merchants bank in this city for the past eleven years, was recently promoted to the position of manager of the new branch, to be opened at Edmonton. Mr. Wilmott is deservedly esteemed by the officials of the bank and for the past few years has been assistant manager in the local branch. Last evening at the P. R. E. depot Mr. and Mrs. Wilmott were given a farewell by a large number of their friends, who united in wishing them every success in their new home.

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OUTFIT HERE.

EDMONTON, Feb. 9, '98.

EDITOR BULLETIN.

I wish to say a word in favor of Edmonton as an outfitting point for gold seekers.

Our party, from Chicago and numbering four men, has outfitted here and found it to be every way satisfactory. Prices and quality are right and goods are in sufficient quantities to satisfy all the requirements of any prospecting party. The weather up to date has been very pleasant, thermometer never going below ten and we camp out without the least discomfort. The weather, generally, we find more pleasant than it would be in Chicago at twenty above.

I would not only advise parties to outfit here, but I would strongly recommend them to order before hand as we did and so avoid any possible delay. Our order was carefully filled and we are thoroughly satisfied with our outfit.

Yours sincerely,

WM. F. KAEKES, Sec.

For the company.

BIRTHS.

KENNEDY—At Victoria, Alberta, on Feb. 7th, the wife of Geo. Kennedy, of a daughter.

## MORE MERIWETHER.

Messrs. W. H. Best and John Enright, two of the members of Meriwether's party, and who, according to Meriwether's words, were "destitute and starving" beyond the pale of civilization, arrived in town on Monday from Grand rapids, both looking the picture of health and they state that their looks do not belie their feelings.

Both men corroborate each other in the story they tell of the tactics used by their late companion in his attempts to procure money from them, and afterwards from their families, on the pretext of rescuing them—when they were never lost, and carrying relief to them—when they never needed it.

Of their trip down the river and subsequent life in their cabin at the rapids, 180 miles below the Landing, they state: "We got down to the Grand rapids with only one trifling accident, and that was when our boat struck a rock in the river and punched a small hole in the bow. We got it off after an hour and a half and found the damage was only slight so we mended the hole and continued the journey. We had Thomas Bowen for a guide and he got us through all right except for the one little mishap for which we could not blame him. At the Grand rapids the water was too low to permit of further travel so we went into winter quarters there. We had never had a settlement with Meriwether as the money expended in outfit and dissatisfaction across which was not lessened when we at last discovered that three of us, namely W. H. Best, John Enright and Moses Leibman had put up all the money and Meriwether nothing. On discovering this we parted company. This was early in December, and the three of us, Best, Enright and Leibman, went into winter quarters in the old police post, five miles down the river from where the remainder of the party were camped. Here we stayed comfortably housed and with plenty of supplies and passed the time in hunting fresh meat, of which we always had a supply. We got one moose which weighed as near as we could estimate, 900 pounds. Meriwether was beautiful and we were "happy as lords." Soon after the division they state, Meriwether left for the Landing without letting them know he was going. Of the lake which he was trying to work in Chicago, they did not hear until Insp. Routledge passed down with his police patrol, and came to investigate their condition when, to show that they were very much alive, they treated him to apple duff with brandy. The next word they heard of Meriwether was his starving condition and had given him \$100 and were to give him more to get them out to civilization. They sent out letters explaining their condition by the returning relief party on the 14th of January, and witherpoor (alias Hall) another member of the party, and brother-in-law of Meriwether's, also returned with the party. This left F. A. Stevens, the last member of the Meriwether faction alone, and he too started back on the 19th of January, and Best and Enright thinking they had better come out and pose their own mail and so end any more swindling, followed on the 20th, and passed Stevens on the trail. As far as the Landing they pulled a toboggan with 200 pounds of food and bedding, and the trip to town occupied all seventeen days. For the first few days they pulled the big dog sled, but the loaded toboggan, but the snow got too deep for him. They made fifteen miles a day and camped out at night in the open without discomfort. Moses Leibman remained behind to hold the camp down during their absence. As to supplies Mr. Best states they have plenty except perhaps flour and they will take about ten sacks back with them. They do not intend to return to the rapids for a week or two and will probably be accompanied by several friends from Chicago, who intend taking the water route when spring opens.

As to life in the woods Mr. Best says: "I never knew what it was to enjoy life and perfect health before. Living in the open air gives a man wonderful vitality and as for appetite—we eat four meals a day and have lunch in between, and then don't seem to get enough."

## THROUGH TO THE NELSON.

At a meeting of the council held last night a request was brought up by Benj. R. Sears, representing the Fresno party, for a guide to be furnished in party by the town, to conduct them from Fort St. John to Fort Nelson, where navigable water commences on that river. The distance is, approximately, 200 miles, over a country never travelled before except by Lady and H. Burbank, a free trader of St. John, who returned with Mr. Sears, was also present and guaranteed to furnish a guide who would take the party through, for \$130. The proposition was accepted by the town, money to be paid Burbank on receipt by the council of a voucher from the leaders of the Fresno party proving that the guide had taken them successfully through. Burbank guarantees to take them through before the snow leaves and to blaze a trail for other parties to follow. This will give a through trail, summer or winter, from Edmonton to the Nelson. A full report will appear next issue.

"I Cannot Sing the Old Songs"

Well, get the latest, and to suit your voice.

Anthems, Glee, Dance Music and Piano Solos of all kinds. When not in stock will procure on very short notice. Mail orders promptly attended to.

A. G. RANDALL.

Office at Town Hall 10 a. m. to 1 p. m.

At Larue & Picard's store 2 to 6 p. m.

## Klondike Sled

With runners. Can be changed in three minutes into a FLAT SLEIGH.

Traineaus, Carts, Buggies.

Combined Sleighs and Carts made specially for the Yukon trail.

Boats of any style

MADE TO ORDER.

Wagons and all Transport

Outfits for summer and

winter travel.

CITY CARRIAGE WORKS,

JOHN KELLY, PROPRIETOR.

## KLONDYKE CLOTHING HOUSE.

Parties going North will find it to their advantage to see our offerings before buying their outfits of Clothing, Boots and Shoes.

We only sell articles of wear, and endeavor to get and give the best value for money. We do business for Cash, and on the basis of "Small Profits and Quick Returns." Goods guaranteed as represented and one price to all.

Inspect our stock and compare prices.

## CLOTHING.

Heavy Mackinaw Suits.  
Heavy Duck Suits (Mackinaw lined),  
Heavy Freize Suits (Moleskin lined),  
Best Corduroy Suits,  
Heavy Duck Overalls,  
Heavy Duck Overalls (lined),  
California Jean Pants,  
California Jean Coats,  
Mackinaw Pants.  
Corduroy Pants,  
Leather Coats (Corduroy lined),  
Leather Coats (Sheepskin lined),  
Duck Coats, reversible,  
Fur Coats.

## FURNISHINGS.

Heavy Ribbed Underclothing,  
Heavy Ribbed Underclothing (Scotch),  
Arctic Underclothing,  
Silk Underclothing,  
Mackinaw Shirts, ribbed Collar and Cuffs,  
Moleskin Shirts,  
Tweed Flannel Shirts,  
Heavy Ribbed Sweaters,  
Heavy Wool Socks,  
German Duffle Socks,  
Woolen Mitts,  
Woolen Gloves,  
Silk Gloves,  
Buckskin Gloves (lined),  
Calfskin Gloves (unlined),  
Horsehide Gloves (lined),  
Buckskin Mitts,  
Wool Wristlets,  
Storm Hood,  
Silk Mufflers,  
Cloth Caps,  
Fur Caps.

## BOOTS & SHOES.

Miners' Split Shoes,  
Miners' Kip Shoes,  
Miners' Milwaukee Calf Shoes,  
Miners' Prospecting Boots (high cut),  
Miners' Boots (long leg),  
Miners' Knee Rubber Boots,  
Miners' Hip Rubber Boots (snag proof),  
Miners' Hip Rubber Boots (leather sole, nailed),  
Moose Skin Moccasins,  
Moose Skin Moccasins (sinew sewn),  
Calf Skin Moccasins (fur lined),  
Dolge Felt Shoes (frost proof).

## EXTRAS.

Blankets, 8, 10 and 12 pounds,  
Sleeping Bags,  
Sleeping Toggles.

We have everything and anything you need in this line and at prices pleasing and profitable to the purchasers.

## GOOD GOODS OUR SPECIALTY.

W. T. Henry & Co.,

HUMBERSTONE BLOCK,

Edmonton, N. W. T.



# Hudson's Bay Company

(INCORPORATED 1670)

Carry the largest and most complete stock of General Goods in Edmonton. Special additions have been made to meet the rush to the Gold Fields, and you will find them prepared to

Fill Prospectors' Orders for Supplies of all kinds

Some reasons why you should outfit with us are:

We have had a large experience in the country, and this we place at your service.

We handle only the best and most reliable goods. We import them direct under the most favorable conditions, and being the largest buyers, we secure the best prices, of which we give our customers the benefit.

We can handle your whole order, not only saving you trouble in the matter, but securing to you the utmost commercial advantage which can be attracted to an extensive transaction.

We have a large connection in the North, and interest in our customers does not end with the sale.

Assistance and information can be secured at any of our posts en route.

Price Lists Supplied on Application.

You will never regret outfitting at

HUDSON'S BAY Stores.

## TO THE YUKON.

(CONTINUED FROM PAGE ONE)

### ADVANTAGES OF THE EDMONTON ROUTE.

The great features in favor of the Edmonton route are that: It starts from a source of cheap supply of food and all miners' requirements.

Pack horses, which form the chief means of transportation over it, are available at lowest prices and in unlimited numbers.

The region to be travelled furnishes feed in summer for pack animals throughout its entire length.

It is a region of moderate climate throughout, resembling that of the Saskatchewan valley.

There are no mountains to be climbed, as the mountain ranges are passed in the river valleys which are followed.

The season of travel opens earlier and closes later on this than on any other route.

There is no question of customs charges from Edmonton to Dawson. It is entirely within Canadian territory.

Help for the trip can be hired cheaply at this end of the route.

While the Pelly and Liard river regions offer the greatest present attractions to prospectors, they are of such a nature that pack horses can be used freely in prospecting if desired, as they are outside the limit of perpetually frozen soil.

When at Pelly river the whole Yukon district as at present known can be reached by excellent down stream navigation, unimpeded by rapids or lakes.

Compare these several points with the facts on any of the coast routes, as they existed last season and will exist during the greater part of the coming summer.

### COMPARISONS.

The only point that can be urged against the Edmonton route is its length from steam communication as compared with the coast routes. But mere length is not the only or chief consideration. The routes by the mouth of the Yukon or by the Mackenzie are the longest possible, and yet they are cheaper than any of the coast routes for the transport of supplies or men.

The Chilkoot pass route is unquestionably the best of the coast routes in use last summer. On that route goods had to be packed on men's backs for 11 miles. From 50 to 75 pounds was a load. A miner's outfit and supplies for a year will weigh 1,300 pounds. If a man undertakes to pack this across himself he will have to make 26 trips of 11 miles with 50 pounds on his back and 26 trips of 11 miles without a load. Each trip occupies a full day. So that he would be 52 days packing his supplies across that 11 miles and would walk 572 miles, all the time over a terrible trail and half the time with a heavy burden on his back. If he does not do this himself he must pay someone else to do it for him. Naturally, the charges are proportioned to the slowness of the service, and last season when the rush commenced the cost of packing across the Chilkoot pass was 50c. per hundred pounds. There is no reason to suppose that the charge next spring will be any less, but rather more on account of the greater rush. The lakes in which the Lewes branch of the Yukon head at the Chilkoot pass do not open until the middle of June, so that the trip down the river cannot be commenced before that time. When the pass has been surmounted, the lakes have opened, and boat has been taken down the Yukon, 300 miles of upstream navigation from the junction of the Pelly and the Lewes would be required to reach the point on the Pelly at which it is reached by the Edmonton route.

The White pass is absolutely condemned by the experience of last season.

The Chilkat has never yet been attempted for the transport of supplies.

The Teslin lake trail is in the same position, but is said to be suitable for pack horses. This is questioned unless very great improvements are made in it, but there is no question, on the evidence of Surveyor Ogilvie, that the feed along it is so limited that feed for pack horses used on it would have to be brought in. As the horses are not there and would only reach there after being purchased on the ranges east of the Rocky mountains, after a long railway travel, and a longer steamboat haul, they would certainly be very dear on arrival and it is not to be expected that packing charges for the 150 miles would be much less than on the Chilkoot route, or that there would not be as much danger of congestion and subsequent stoppage of traffic. The same remarks as to late opening of the lakes and the upstream navigation to reach the Upper Pelly apply to this route as to the Chilkoot pass. The possibility that a railway may be built over this route does not affect the case as far as the early travel of next season is concerned; unless it might be to still further congest the route and increase the cost of transport. The same would apply to the Chilkoot and White passes.

### TABLE OF DISTANCES.

All land travel:	Miles.
St. Albert,	9
Pembina river,	36
Athabasca at Fort Assiniboine,	30
South shore Lesser Slave lake,	85
Hudson's Bay Co's post,	40
Peace river,	60
Battle river,	60
Hay river,	120
Nelson river forks,	60
	500

Land or water travel:	Miles.
Nelson river Forks to Liard river crossing, below canon,	135
To head of Devil's portage,	35
" Foot of Brule portage,	40
" Head of Cranberry portage,	15
" Junction of Dease,	60
" Frances lake at mouth of Finlayson creek,	115
" Finlayson lake,	30
" Head of lake,	7
" Pelly river,	13
	450
Total distance, Edmonton to Pelly river,	950

### CHARACTER OF ROUTE.

Edmonton to Pembina river: Good wagon road through farming settlement. Good feed.

Pembina to Athabasca: Wagon road through partly timbered unsettled country. Good feed.

Athabasca to Slave river: A road has now been opened by the Northwest Government to within 20 miles of Swan river. The road will be completed to Swan river by the 20th inst. and before this paper gets well before the public. This will give a through winter road 12 feet wide for bobsleighs from Edmonton to Ft. St. John. From the end of the cut trail the winter road is down the ice on Swan river to Slave lake, and thence westward. Feed fairly good. See letter of Fugard party in another column.

South shore Lesser Slave lake to west end of lake: Open country, very rank grass. Excellent feed.

Lesser Slave lake to Peace river: Wagon road in constant use. Good feed.

Peace river to Battle river: Good pack trail through partly open country. Excellent feed.

Battle river to Hay river: Partly open country, trail not so well defined. Good feed.

Hay river to Forks of Nelson: Country chiefly covered with small timber, old trails gone out of use. Feed good in openings.

Plenty of water all the way.

From the Forks of the Nelson to the Pelly river the country is chiefly timbered, and feed is consequently not so abundant as in the more open country, but still is ample for summer travel. The route follows the valleys of the Nelson and of the Liard, keeping on the high ground from the Forks of the Nelson along the foot hills lying west of the river.

As the country is more rugged south than north of the Liard that river would be crossed below the canon. There is good navigation from the Forks of the Nelson to the foot of the canon on the Liard. Therefore, boats might be built at the Forks and the supplies taken in them to the foot of the canon, while the horses were driven overland without loads to the same point.

The horses could be used to portage the supplies up the length of the canon, 35 miles, while the boats were taken up light.

In the succeeding 40 miles to the foot of Brule portage the navigation is

(CONTINUED ON PAGE FIVE.)

## The All-Canadian Route

(NO CUSTOMS DUTIES.)

### Edmonton to the Klondyke

...BY LAND OR WATER...

And the Peace, Liard and Pelly River Gold Fields.  
THE ONLY ROUTE OPEN TO TRAVEL ALL THE YEAR ROUND.

## ROSS BROS.

HARDWARE AND GENERAL MERCHANTS

And Miners' and Prospectors' Outfitters.

WE HAVE ALL A MINER WANTS—EXCEPT GOLD.

### Outfits One Man One Year.

Provisions, \$85.00 to \$125.00

Flour,	Currants,	Evaporated Apples,
Bacon,	Raisins,	Evaporated Peaches,
Beans,	Split Peas,	Evaporated Apricots,
Tea,	Pot Barley,	Evaporated Prunes,
Coffee,	Hardtack,	Evaporated Vegetables,
Sugar,	Mustard,	Evaporated Onions,
Chocolate,	Pepper,	Dessicated Potatoes,
Cocoa,	Ground Ginger,	Compessed Soups,
Butter,	Baking Powder,	Condensed Milk,
Lard,	Baking Soda,	Evaporated Cream,
Corn Meal,	Yeast Caeks,	Condensed Vinegar,
Oat Meal,	Salt,	Candles,
Rice,	Matches,	Scap.

Condensed Bacon, Army Foods and Emergency Rations; Canned and Pressed Meats and Lime Juice Nodules.

Clothing, \$85 to \$125.

Arctic Underwear,	Flannel Shirts,	Sweaters,
Arctic Socks,	Tweed Shirts,	Jerseys,
Arctic Mitts,	Mackinaw Shirts,	Cardigans,
Reversible Jackets,	Mackinaw Pants,	Moccasins,
Reefer Jackets,	Mackinaw Suits,	Suspenders,
Mackinaw Jackets,	Corduroy Suits,	Towls.
Gum Boots with and without leather soles.	Miners' high and low Leather Boots.	Rubber Coats.
Oilskin Slickers.	Waterproof Coats.	
Duck Overalls, lined and unlined.	Duck Smocks, lined and unlined.	
Corduroy Hats and Caps.	Fur Storm Caps.	Felt and Canvas Hats and Caps.
Heavy wool and leather Gloves and Mitts.	All-wool Blankets.	Dunnage Bags.
Oil and rubber bed sheets,	Sleeping bags,	Handkerchiefs,
	Mufflers,	Duffle and Stroud.

Hardware, \$40 to \$75

Knives and Forks,	Tea and Tablespoons,	Basting Spoon,
Fry Pan,	Breadpan,	Dutch Oven,
Butcher Knife,	Sheath Knife,	Miners' Canteen, Razor Strop,
Pocket Knife,	Scissors,	Hunter's Axe and Sheath,
Miners' Coffee and Tea Pots,	Miners' Pan,	Prospector's Hand Pick,
Miners' Shovels,	Miners' Drills,	Miners' Bailers,
Drifting Pick,	Safety Fuse,	Fibre Tubs,
Blasting Powder,	Quicksilver,	Chamois,
Grizzley Irons,	Magnets,	Magnifying Glass,
Mining Blanket,	Candle Lantern,	Miners' Candlestick,
Miner's Lamp,	Mariner's Compass,	Fire Steel and Flints,
Gold Scales,	Canoe Adze,	House Adze,
Chopping Axe,	Claw Hatchet,	Jack Plane,
Nail Hammer,	Rip Saw,	Whip or Pit Saw,
Cross-cut Saw,	Spoke Shave,	Spirit Level,
Drawing Knife,	Steel Square,	Pocket Rule,
Broad Axe,	Screwdriver,	Cold Chisel,
Monkey Wrench,	Eye Augers,	Brace and Bits,
Framing Chisels,	Axe Stone,	Files,
Oil Stone,	Rope,	Blocks and Tackle,
Steel Measuring Tapes,	Calking Irons,	Calking Mallets,
Pitch, Oakum,	Wire, Cut and Boat Nails,	Electric Solder,
Bolts and Rivets,	Wood screws,	Steel Traps,
Fishing Nets,	Fishing Twines,	Snare Wire,
Fishing Tackle,	Tracking Lines,	Water Pails,

Wall, Wedge and Miners' Tents.	Beet and Wagon Covers.	Plain Duck.	Oiled Duck, 56in.
Covering Sacks.	Rifles.	Revolvers.	Shot Guns.
Fixed Rifle, Revolver and Shot Gun Ammunition in all makes and calibres.			Reloading Tools.
Shot.	Bar Lead.	Gun Caps.	Gun Covers.
Powder.	Prospectors' Knapsacks.	Horse and Cattle Bells.	Cartridge Belts.
Money Belts.	Thermometers.	Rubber Cement.	Rectrics.
Hand Bellows.		Snowshoes.	Crucibles.
			Washboards.

Miners' Medicine Chest \$4 to \$8

Compact, nothing to freeze. All drugs in powder and tablets. Put up by reliable chemists. Remedies for all complaints, snow blindness, sore eyes and frost bites. Worth double the price if purchased in the ordinary way. If desired, can purchase Boats, Horses, Harness, Pack Saddles, Sleds, etc., etc., and hire Guides. Reliable information furnished by an ex-H. B. Co. officer of over thirty years' experience in the North-West. Correspondence solicited and enquiries promptly answered. We have agencies at the Athabasca Landing, Peace River and throughout the North. Latest official maps and reports of all routes kept on file.

ROSS BROS., EDMONTON, ALBERTA.

(ESTABLISHED 1883.)



# St. Albert

- TO THE -

## - Yukon -

### ST. ALBERT

Is a good business town nine miles north-west of Edmonton, directly on the road to the Great Gold Fields of the Peace, Liard and Pelly rivers and beyond.

There are several large establishments here, including Stores, Butchers, Hotels, Blacksmiths and Freighters.

Intending Gold-seekers are advised to come here, for living is cheap, reliable information from the natives of the country may be had for nothing, and this is the last outfitting point for the Klondyke.

Packhorses to any number can be bought here cheap, and encampments with abundance of wood and water are to be found everywhere.

### H. W. McKenney

YUKON OUTFITTER

And Mammoth Wholesale and Retail General Store,

ST. ALBERT, N. W. T.

Dry-Goods,

Clothing,

Mitts, Gloves,

Boots, Shoes,

Hats, Caps

Hardware,

Crockery,

Tinware

### Miners' Supplies

OF ALL KINDS.

Flour, Miners' Picks,  
Tea, Miners' Shovels,  
Sugar, Miners' Spades,  
Bacon, Gold Pans,  
Beans, Rope,  
Rice, Axes,  
Salt, Hatchets,  
Oatmeal, Saws,  
Coffee, Tools,

Patent Medicines, Knives,  
Condensed Milk, Powder,  
Canned Goods, Shot,  
Etc. Etc. Etc. Shells.

Telephone connection with Railway

Station and Edmonton.  
Farms for Sale. Write for information

### H. W. McKenney

ST. ALBERT, N. W. T.

HEADQUARTERS FOR

### Medical Supplies

- FOR THE -

### Gold Fields.

Citric Acid, Eye Shades,  
Eye Protectors,  
Goggles—velvet bound,  
CURE FOR SNOWBLINDNESS  
in tablet form,  
... Saccharine Tablets...

Five years' experience in the medical supply Dept. North-West Mounted Police.

Purity guaranteed. Prices right.

### G. H. Graydon,

EDMONTON, ALBERTA.

### KLONDYKERS!

Get your Flat Sleighs and Transport Outfits at headquarters—the City Carriage Works—and save commission.

CITY CARRIAGE WORKS.

### Carts for the Klondyke Trail

To arrive before March

1st, one car of Ox

or Pony Carts.

Now on the way from the factories: Two cars of Drills, Sulky, Gang and Walking Plows, Harrows, Disk Harrows, etc., two cars of Special Teaming and Farm Wagons, one car of Buggies, Buckboards, etc.

Agents for the celebrated McCormick Binders and Mowers, and American and Canadian Threshers and Engines.

THOS. BELLAMY.

### Evaporated Potatoes

For sale to the Trade, at

CAIRNS' New Brewery.

Before going to the..

## Yukon

You can save Gold by buying your supplies at

### Garipey & Chenier

GENERAL...  
MERCHANTS

Dealers in Staple Dry Goods,  
Boots & Shoes, Groceries,  
Flour, Grain, Provisions  
Wall Paper, Crockery, Etc.

### Carriage AND Repair

.. SHOP ..

QUEEN STREET, EDMONTON.

(In rear Jas. McDonald's shop)

.. SEE MY ..

Klondyke Toboggans and Flat Sleighs

at reasonable prices. Repairs of all kinds on the shortest notice.

Low prices is my motto. Come and see me

JOS. BOUGIE.

### ARTHUR COASKE,

(LATE OF CHICAGO)

### Tailor and Furrier

Suits to order at reasonable prices.

Furs remodeled and repaired.

YUKON TRADE A SPECIALTY.

Next door east of Telegraph Office.

### SLEIGHS! JUMPERS!

### Flat Sleighs

150 Toboggans or Flat Sleighs on hand, also Jumpers and Sleighs, Horse and Dog Carriols, Etc.

### J. WALTER,

Upper Ferry Crossing.  
EDMONTON, ALBERTA.

## TO THE YUKON.

(CONTINUED FROM PAGE FOUR)

good, and again the boats could be used to advantage. It is in this stretch the Rocky mountains are crossed.

From the foot of Brule portage to the head of Cranberry portage, it would probably pay best to again use horses either for the whole distance on one trip or on the three portages of two miles, half a mile, and half a mile, respectively.

From the head of Cranberry portage the boats could be used to advantage again, while the horses were driven light overland to the mouth of Finlayson creek on Frances lake.

Although there is a canoe route from Frances lake to the Pelly by way of Finlayson creek and lake and Campbell creek it would probably be found advantageous to use horses for the whole 50 miles if they were available. The feed for horses on this portage is said by Dr. Dawson to be very good.

By taking advantage of the water communication there would only be 100 miles of packing for the horses from the Forks of the Nelson to Pelly river.

If a party had no horses and were good boatmen, not afraid of hard work the total portaging could be reduced to: Four miles at the Devil's portage at the head of the canon of the Liard, two miles at Brule portage, half a mile each at Mountain and Cranberry portages, three half mile portages above the mouth of the Dease, two miles at the mouth of Finlayson creek and four miles from Finlayson lake to Campbell creek. Total, 14½ miles, against 25 miles in the Chilkoot pass, and no mountains to climb. For the whole distance from the Forks of the Nelson to the Pelly river the feed is good; as it is down the Pelly, wherever the country is not too heavily timbered. In the immediate valleys of the Liard and Nelson the timber is large, but on the upland it is inclined to be scrubby, with many openings, offering ample opportunity for horses to get plenty of feed. There are no mountains to climb along this route, and the country is not even rugged.

### AN ALTERNATIVE ROUTE.

From the Forks of the Nelson there is a clear course down the Nelson, the Liard and the Mackenzie to Peel river, without lakes or rapids to impede navigation. The Nelson and Liard open about May 10th, and the Liard opens the Mackenzie from its junction down to its mouth, while the part above the Liard to Great Slave lake and the lake itself remains solid. The lake cannot generally be crossed until the end of June or first week in July. By going to the Forks of the Nelson on the snow and being ready to take boat as soon as the river opened the prospector could reach Peel river nearly two months sooner than from the Athabasca Landing—could in fact be at Ft. Yukon before the parties now wintering on the south shore of Great Slave lake could start across the lake. No doubt parties desiring to take this route could get their supplies freighted to the Nelson river this winter at a cost not exceeding 10c. a pound.

### THE EARLIEST ROUTE.

The trip from Edmonton to Pelly river will, doubtless, take from two to three months with loads under favorable circumstances. If haste is desired a start can be advantageously made from Edmonton in the latter end of February or early in March, and the first 500 miles to the Forks of Nelson river, as shown on the accompanying map, covered on the snow. This part of the journey could be completed by April 1st or 15th, and would enable the prospector to reach Pelly river by boat or pack horse by the 1st of June. The Pelly is open at that time and boat could be taken direct down stream to Dawson if desired. The lakes at the head of the Lewes, reached from the Chilkoot and White passes, are not open until the middle or end of June, so that the traveller from Edmonton would have at least two, and possibly four, weeks advantage in reaching Dawson over the traveller going by the Chilkoot pass, besides having a much larger quantity of supplies. There are many advantages connected with starting thus early besides the earlier date of reaching the Yukon. The Athabasca and Peace rivers would be crossed on the ice, much heavier loads could be hauled by the horses on sleighs than could be packed on their backs, and better time could be made. On the other hand is the expense of purchasing feed for the horses while collecting them and on the journey, and the cost of the sleighs and harness, which would be of no further use after the snow had gone.

The trip to Nelson river should be made in 30 days at 16 miles a day. There is no doubt about the practicability of the trip if there is a large party, even though there had been no other travel over the road, as the lead horses could be given light loads and be changed continually so that the labor of breaking the road would fall upon the whole number equally. There is very little doubt, however, that the road will be kept open all winter at least to Peace river, which is over half way to the Nelson. It would not be desirable for the ordinary prospector to push far beyond the Forks of the Nelson on the snow, as the winter grazing for horses is believed not to be so good beyond that point, and the amount of horse feed that would consequently have to be carried would reduce the amount of supplies below the profitable point.

### WINTER FREIGHTING.

The extra cost of an outfit to start on sleighs in March will be about as follows:

Flat sleigh,	\$8 00
Harness for horse hitched to sleigh,	3 50
300 lbs. oats per horse (10 lbs. a day for 30 days) say,	4 00
	\$15 50

A horse should haul 700 pounds on a flat sleigh, of which 300 pounds would be his own feed and 400 load. If a very large number of horses were being taken double sleighs could be used, in which case the horses could start with 1,000 pounds each, or 700 pounds each of load. As the maximum load for a pack horse on a long journey is 200 pounds, one horse can haul to Nelson river the loads of from two to four pack horses. To put it another way. By starting from Edmonton in March and using the boat route from the Forks of the Nelson, less than half as many horses will be required to transport a certain quantity of supplies than by pack trail in the summer season. Five horses will land 2,000 pounds of supplies at Nelson river on sleighs. Ten as good, or better, horses would be required to land the same amount at the same place using packs, and the work of the driver would be much greater.

It is the business of the BULLETIN in this article to give information rather than advice. But having suggested a start in March it would not wish this to be construed into advice to start in December or January, or even early in February. There is nothing in the climate, snow fall or nature of the surface of the country to prevent winter travel with horses along the whole route from Edmonton to Fort Selkirk, provided they can be supplied with food. But it should be distinctly understood that unless arrangements can be made for supplying the horses with sufficient food they simply cannot go. As already stated a horse can haul 400 pounds of load, and oats enough to last him for 30 days. But that is the limit, and it requires a good horse, acclimated, in good condition when starting, and well handled to do this. Beyond 30 days travel, unless other arrangements are made for freighting out a further supply of oats, he cannot go and do good work. On the other hand if oats and hay are provided he can go all winter. If he goes for 30 days and is played out by his exertions, or if no provision is made to feed him for the rest of the winter, unless what remains of it is short and mild he will either die or he will be too poor to do any work in the spring. The idea of starting in March is that the feed which he will haul will last him until the snow leaves the ground sufficiently to allow him to get grass. In another winter when the route shall have been established and hay put up at intervals of say 50 to 100 miles, there will be no serious difficulty in travelling right through at any time of the winter. But that is not now.

### PEACE RIVER.

The Peace river is reached on the overland route to the Yukon 260 miles from Edmonton. To this point freight is taken from Edmonton in summer at a cost of five cents a pound, and there is no doubt that contracts can be made at the same rate for winter freight. As already mentioned the Finlay and Parsnip rivers which form the Peace and the Omicron tributary of the former, are known to be richly gold bearing. There is on these rivers, and particularly on the Finlay, a large area of gold bearing territory that has never been prospected or even explored, owing to the cost of getting in supplies from the coast. These rivers are very much more accessible from the east than the coast. These rivers, particularly in summer, as there is good boat navigation all the way from Peace River Landing to the Finlay, except at the Rocky mountain portage, 12 miles in length. The country is suitable for the use of pack horses all the way to the Finlay, and the distance from Peace River Landing to the Finlay is about 250 miles. Up the Finlay, west of the Rocky mountains, is a region extending to the celebrated Cassiar diggings which has never been prospected. In this district is situated the Mud river which flows into the Liard and is said to be rich in gold. This region is very widely attracting the attention of prospectors at present. It is much more accessible than the Yukon and is on the way there, if it is desired to take that route.

(CONTINUED ON PAGE SIX.)

.. TO THE ..

## .. Klondyke ..

VIA EDMONTON

Is the best route to the Gold Fields, and is open to travel the whole year round. By this route you can reach and possibly strike it rich on the following

- - Gold Bearing Rivers - -

Liard, Peace, Nelson, Pelly, Hay and numerous other streams.

## Larue & Picard

Are prepared to furnish Miners and Prospectors all the goods necessary for their

- - OUTFITS - -

New Goods coming in by every train, such as

Mackinaws—	Slickers,	Miners' Boots
Jackets,	Blankets,	Rubber Boots
Pants,	Duck,	Moccasins,
Shirts,	Duffel,	Mitts, Etc.

Evaporated Potatoes and choice Evaporated Vegetables. Large stock of Groceries and Provisions put up specially for Miners and Prospectors.

Branch Store at Lesser Slave lake with good general stock, also Fresh Meats, and Hay and Grain for horses. Ask for our Pocket Guide of useful information

.. OUR MOTTO ..

### Quick Sales and Small Profits

## LARUE & PICARD,

General Merchants and Outfitters, Edmonton.

## EDMONTON SADDLERY CO.

HEADQUARTERS FOR TENTS,

Pack Saddles, Cart Harness,

Combined Pack Saddles and

Flat Sleigh Harness,

Waterproof Sheets, Sleeping Bags.

Also Flat Sleighs and Sleighs

of every description.

Bring your horses to us and we will fit you ready for the road.

W. H. MARTIN, Manager.

### Fire Arms for the Klondyke



Winchester and Marlin Rifles in every calibre made. Lee Metford and Savage .303 Rifles. Combined Rifle and Shot Guns. Colt's Frontier Revolver with 7½ inch barrel, using 44 Rifle or Shot Cartridges.

Greener, Scott, Clabrough and other good English Shot Guns. Ammunition of all kinds. Catalogue on application. Mail or wire orders promptly attended to

CHARLES E. TISDALL, Vancouver, B. C.

### Time Flies

And I have all the latest styles of Clocks and Watches to mark its flight.

My stock of Watches, Clocks, Jewelry and Silverware for 1898 is most complete. Microscopes and compasses in great variety, also interchangeable spectacles, guaranteed to suit all eyes.

E. RAYMER.

Skilful repairing of all kinds a specialty.

### Two Cottages and Four Main St. Lots

.. FOR SALE ..

(1) One or both of the cottages opposite the residence of D. W. MacDonald on McDougall Street.

(2) Lots 19 and 20 on Jasper Avenue in River Lot 6, commencing 50ft. west of the Massey-Harris warehouse; will be sold in lots from 25ft up to 100ft as desired by purchaser. For terms apply to

H. C. TAYLOR, Barrister,  
OWNER'S AGENT.



## JOHN F. FORBES

ACCOUNTANT,  
Customs and Real Estate  
Broker.

Advertising space in Jasper Hotel for rent.  
Office over Post Office, Edmonton.

## Stock-Taking Sale

Great Slaughter of Winter  
Goods.

Must make room for New Stock. For the  
next thirty days we will offer our  
Winter Goods at greatly reduced prices.

Call and inspect our Stock of Hardware  
...TERMS CASH...

SHERA & CO.,  
FORT SASKATCHEWAN.

TELEPHONE ..

Baldwin &  
Goodridge

For first class Coal. \$2.00 per ton. No  
credit.

EDMONTON DYE WORKS  
—F. Mayerhofer—

PROPRIETOR.

Near Electric Light Works.  
All orders promptly attended to and first-class  
work guaranteed.

ARCHITECTS.

EDMONTON & PATER,  
Architects, draughtsmen and valuers  
OFFICE—Imperial Bank Building.  
W. S. EDWARDS, Architect and Valuator.

AUCTIONEER.

W. S. ROBERTSON  
AUCTIONEER AND VALUATOR.  
Telephone Communication. Sheriff's Office.  
Country sales attended to. Charges moderate.

GEO. T. BRAGG,  
C.P.R. Land Agent

Fire, Life, Accident, and Plate Glass Insurance.  
Customs Broker, etc.  
Office first door west of Blower's Furniture Store.

Watch

for

J. Looby's

Harness

Advertisement

Next

Issue.

NOTICE.

Notice is hereby given that an application will be  
made to the Parliament of Canada, at its next  
session, for an Act to incorporate a company with  
power to construct, maintain and operate a railway  
from a point at or near Edmonton, in the District of  
Alberta, in the Northwest Territories of Canada, to  
a point on the Peace river, known by the most  
feasible and practicable route to a point in the  
Yukon District; also, to construct branch lines  
thereof; also to own and operate steamers, barges,  
ferries and other vessels in connection with such  
railway; also to maintain roads and to do a general  
transportation business; also to build and operate  
tramways between such points as may be desirable  
between the aforesaid points, or points on other  
northern waters; also to erect and maintain bridges  
across rivers and streams on such roads as they may  
operate upon, with privilege to charge tolls therefor  
as may be prescribed by law; also to do a general  
trading and supply business; also to buy, sell and  
develop mineral lands and mineral claims of all  
kinds, together with such other powers and privi-  
leges as may be necessary or incident thereto.

Dated at the City of Calgary, in the District of  
Alberta, this 27th day of December, A.D. 1907.

LOUGHEED & BENNETT,  
1907  
Advocates for the Applicants.

NOTICE.

Notice is hereby given that application will be  
made to the Parliament of Canada, at its next  
session, for an Act to incorporate a company to  
mine, crush and smelt gold and other minerals in  
Canada, with power to enter into agreements with  
the Governments of Canada to conduct mining opera-  
tions upon altered or other mining locations, the  
property of the Crown, for the joint account of the  
Government and the company.

GEMMILL & MAY,  
1907  
For the Applicants.

NOTICE.

Is hereby given that an application will be made  
to the Parliament of Canada at the next session there-  
of for an Act to incorporate a company to construct,  
operate and maintain tramways for the carriage of  
passengers, goods and boats over the portage at  
Millet Canyon and White Horse rapids respectively,  
on the Lewis river, N. W. T., and to collect tolls  
and to construct landing stages and to do all things  
necessary and convenient for the carrying of pas-  
sengers, goods and freight, and to operate the said  
tramways by electricity or steam or other motive  
power, as to the company may seem best and to  
take as much of the waters of the said river  
as may be necessary to generate the electricity re-  
quired and to build all necessary dams, flumes and  
aqueducts, etc., etc.

BODWELL, TRYING & DUFF,  
14-31  
Solicitors for the Applicants.  
Victoria, B. C., 8th December, A.D. 1907.

NOTICE.

..ALBERTA..  
Meat and Packing  
..HOUSE..  
Capacity 200 Hogs a day.  
Pork Packer and dealer in all  
kinds of Fresh and Cured Meats.

WHOLESALE AND RETAIL.  
Special attention paid to Miners  
Supplies.

Co'd Storage and Packing House—East  
Edmonton.

Office and Store—Jasper Ave., Edmonton.

C. GALLAGHER,

## OPENING OF PARLIAMENT.

The third session of the eighth parliament  
of the Dominion of Canada was opened  
with the usual formalities on the afternoon  
of the 3rd inst. In the speech from the  
throne, His Excellency, the governor-general,  
touched upon the remarkable advance  
in the political importance and material  
prosperity of Canada during the year which  
had just closed. The matter of the loan  
recently effected was referred to as show-  
ing that the credit of Canada had never  
stood higher in European markets and also  
as an assurance that the rate of interest on  
our national indebtedness would be reduced.  
Continuing, he congratulated the House on  
the warm appreciation manifested every-  
where throughout the mother country in  
reference to the conduct of Canada in the  
Dominion.

The action of the Imperial government in  
denouncing the treaties with Germany  
and Belgium, also affords most satisfactory  
evidence of their desire to facilitate your  
efforts to promote the closest possible com-  
mercial relations between Canada and the  
remainder of the Empire, and will, I trust,  
contribute materially to the development  
of Imperial trade.

The extraordinary gold discoveries recent-  
ly made upon the Yukon and its tributaries,  
appear likely to result in an enormous  
influx of people into that region, and have  
compelled the government to take prompt  
action for the preservation of law and  
order in that distant and almost inacces-  
sible locality; measures will be laid before  
you for that purpose.

A contract has been entered into, subject  
to your approval, for the completion at the  
earliest possible moment of a system of rail  
and river communication through Canadian  
territory with the Klondike and principal  
gold fields, which it is expected will secure  
to Canada the larger portion of the lucrative  
traffic of that country.

The bountiful harvest with which we  
have been favored by a benevolent pro-  
vidence has contributed greatly to the in-  
crease of our prosperity, and I am glad to  
note that the trade and commerce of the  
Dominion, and more especially the amount  
and value of her principal exports, have in-  
creased greatly during the past eighteen  
months, and there is good reason to believe  
that this improvement may be maintained  
if not augmented, during the remainder of  
the present year.

WANTED.  
Bull, five years old, Shorthorn; want to mutual  
exchange. Apply  
HARRIS & SONS,  
30-33

WANTED.  
Teacher for the Stony Plain Public School, No.  
322, holding first or second class certificate. Duties  
to commence about 15th March and to continue for  
9 months. Apply, stating experience and salary ex-  
pected, to the undersigned.

GEO. SUTHERLAND,  
Secretary.

A Good  
Opportunity

TO PROCURE

Good Clothing  
Cheap

Having made arrangements for one of our  
representatives to go to Montreal for the  
purpose of buying fresh stock, etc.,  
we are desirous of reducing our present  
VERY LARGE STOCK OF CLOTHING  
and in consequence will offer to the  
public the

..PICK OF 250 SUITS..  
Comprising all kinds and qualities of  
Men's Suits, upon which we will make  
LARGE REDUCTIONS.

Give us a trial and you will be fully  
convinced that we are carrying out  
what we advertise.

Sigler & Cristall

Next to Macdonald's Drug Store.

La Banque Jacques  
Cartier,

JASPER AVENUE, EDMONTON.

POST OFFICE, SOUTH EDMONTON

J. E. LAURENCE, Manager

HAYMARKET....

RESTAURANT

Meals at all hours and lunch put up to  
order. Lodging in connection. Terms  
right.

R. MATZ,  
22 North of McCauley's Stables.

The Edmonton  
Flour Mills - -

The undersigned beg to notify the public  
that they have leased the above flour mill  
from D. R. Fraser for the season. The ma-  
chinery has been recently overhauled and  
put in first-class shape and they are now  
prepared to do first-class work.

GRISTING & CHOPPING

Done on short notice.

J. S. McCALLUM,  
P. BLATCHFORD.

G. W. R. ALMON

Real Estate, Fire and Life Insurance Agent,  
Bainbridge Block, Edmonton.

COMPANIES REPRESENTED:  
London & Lancashire Fire Insurance Co.  
Maritima Fire Insurance Co.  
Imperial Life Assurance Co.

BAPTIST CHURCH—Services, Sunday 11 a. m.  
and 7 p. m. Sunday School 3 p. m. Prayer  
and social meeting, Wednesday, 8 p. m. Strangers  
welcome. C. B. Freeman, Pastor.

BUILDING.  
J. A. McDONALD, Builder and Contractor. Sash  
and Doors on hand and made to order. Plans and  
estimates of buildings furnished. Office and shop  
junction Jasper Avenue and Main street. P. O. Box  
107. JAMES McDONALD

## TO THE YUKON.

(CONTINUED FROM PAGE FIVE.)

### OUTFIT AT EDMONTON.

As Edmonton is the terminus of the railway and, therefore, the point at  
which the prospector provides or arranges for his own means of transport, it is  
naturally the place at which to outfit for either the land or water routes. As  
the trade of the whole of the Mackenzie and Peace river regions, including  
the Porcupine branch of the Yukon, centres here, it is above everything else  
an outfitting place. That is the business which is the principal cause of its  
existence and prosperity. The gold mining on the Saskatchewan, McLeod,  
Athabasca and Peace rivers for many years past has made the dealers familiar  
with the requirements of miners. Consequently prospectors can be sure of  
getting the articles they require, of the kind and quality they require, at the  
time and place at which they are required, and where they are in the best  
position to make their collection. By far the larger number of parties who  
passed through Edmonton last summer and this winter have outfitted here.  
All have expressed themselves well pleased with the goods and prices. Those  
who have brought their outfits with them, including parties from Montreal  
and Chicago, have in all cases written back to their friends coming afterwards,  
to buy at Edmonton. The Edmonton merchant buying wholesale and bring-  
ing in by the car load can certainly lay down goods here much cheaper than  
the individual buying retail in the east and paying way freight rates on the  
long distance to Edmonton. Competing specially for the outfitting trade, it  
is business for the local merchant to divide this advantage with his customer,  
which he does. So that any goods required for a prospector's outfit, bought  
no matter where, will cost more landed at Edmonton by the prospector than  
he can buy them for here. In the case of purchases in the States the disad-  
vantage is increased by the duty on all articles and by the fact that all woolen  
fabrics are dearer in the States than in Canada to begin with. In the case of  
provisions, such as flour, oatmeal, butter, bacon, lard, etc., as Edmonton ex-  
ports these products they are necessarily cheaper here at the point of produc-  
tion than they can be at points to which they are exported. In the district  
are two large first-class roller flour mills; a large and also first-class oatmeal  
mill; and the largest pork packing establishment west of Winnipeg, a creamery,  
etc. The wholesale outfitting business of the town includes an establishment  
of the Hudson's Bay Company, having the unlimited resources of that com-  
pany at its back to meet any possible demand; McDougall & Secord, who do  
an outfitting business of several hundreds of thousands of dollars a year, and  
who are interested in trading posts as far north as Great Slave lake; Ross  
Bros., who, having established an immense hardware trade, have gone into  
general outfitting on a large scale; Larue & Picard, who have trading estab-  
lishments at Slave lake and on Peace river, and a number of other large estab-  
lishments in general and special lines, who compete strongly for the local and  
outfitting trade, but make a specialty of the former rather than of the latter.  
Besides these Galt Bros., K. Mackenzie, Thompson & Codville, wholesale  
grocers of Winnipeg, have branch establishments at Edmonton. If at any  
time the impossible should happen and such a rush should occur that the de-  
mand could not be met at once, any of these houses can get the goods required  
on an order by wire in less time than the same goods purchased retail in an  
eastern city could reach Edmonton. In several cases this season parties were  
delayed two and three weeks waiting for goods which they had ordered before  
starting, and in several cases had to purchase other outfits at Edmonton rather  
than suffer longer delay. In many cases also men had burdened themselves  
with non-essentials so that they were overloaded, without having a  
full supply of essentials. The freight, which has to be paid from say Mon-  
treal to Edmonton, amounts to from \$2.50 to \$3.75 per hundred pounds. This  
must be added to the cost in the east when making comparison with Edmon-  
ton prices.

Not the least advantage of outfitting at Edmonton is that the prospector  
can suit his purchase to his means of transport, as he cannot before reaching  
here and seeing exactly what is the capacity of his transport. This is par-  
ticularly important if pack horses are used. While it is to the interest of the  
Edmonton merchant to sell the prospector goods it is not to his interest to sell  
him more than he can get through with; therefore, the advice as to what is  
actually necessary that can be had at Edmonton is more reliable than at any  
other point where the dealer has not the same direct interest in the success of  
the trip. The amount and kinds of food required may be judged by taking the  
daily rations supplied the Northwest mounted police, as follows:

### DAILY RATIONS.

	lb.	oz.	lb.	oz.
Fresh beef,	1	8		
Or bacon or corned beef			1	
Bread,	1	8		
Or flour or biscuits,			1	4
Potatoes,	1			
Or dried apples, 3 oz., or beans,				4
Sugar,				3
Rice or barley,				1
Butter,				2
Tea,				1/2
Coffee,				1/2
Salt,				1/2
Total,	4		2	15 1/2

Pepper, soap, candles, matches extra. When on prairie or field duty an  
extra issue not exceeding 25 per cent. is made.

From the foregoing it will be seen that each man is expected to use three  
pounds weight of necessities every day, or 1,100 pounds a year. In travelling  
by boat a wide latitude may be allowed as to supplies, but in packing on horses  
the weight must be kept down, and the packages be easily handled and not  
liable to break. For his own sake the packer's food supply should be confined  
principally to flour, bacon, beans, rice, lump sugar, tea and baking powder.  
These are all easily carried and handled and not liable to spoil. Matches, salt,  
pepper, soap, candles, etc., are only absolutely necessary in small quantities.  
Everything should be of the very best quality and doubled sacked in parcels not  
exceeding 50 pounds. There is no economy in saving a cent or two on the pur-  
chase price of an article upon which dollars are to be expended in freighting.  
This point cannot be emphasized too strongly. In the quotations given below  
only the best quality and maximum price is quoted.

### YEARLY OUTFIT.

Food and sundries for one year's outfit by pack horse:	
450 lbs. flour @ \$2.00 or \$2.25,	\$13 50
350 lbs. bacon, side, @ 11c., (Breakfast bacon, 13)	38 50
75 lbs. beans @ 4 1/2c. per lb.,	3 37 1/2
50 lbs. rice @ 7c.,	3 50
75 lbs. sugar, Paris lump, @ 6 1/2c.,	4 87 1/2
25 lbs. tea @ 30c.,	9 00
12 lbs. baking powder, in tins, 20c.,	2 40
10 lbs. salt @ 2c.,	20
1 lb. pepper @ 20c.,	20
8 lbs., 4 boxes of matches,	60
12 lbs. soap,	1 00
2 lbs. ginger, medicines, etc.,	2 85
1070	\$80 00

The beans and rice may be supplemented or replaced by evaporated fruit or  
vegetables. Sugar is taken instead of syrup, being more easily carried and can  
easily be made into syrup on occasion.

A cooking and mining outfit is necessary as well as food, and the weight  
and cost must be reckoned. To travel or mine successfully two men must go  
together; therefore, instead of each having a separate outfit throughout, the more  
heavy and costly of this class of necessities may be divided between two or even  
four miners.

Hardware, single outfit:

	Weight lbs.	Cost.
Cooking utensils:		
Copper kettles, set of 3,	1 1/2	\$3 50
Frying pan,	1 1/2	50
3 tin plates, 2 tin cups, table knife and fork and butcher knife,	1	1 00
Mining tools:		
Shovel,	4 1/2	1 00
Pick,	4	75
Gold pan,	1 1/2	50
Quicksilver,	1	80

(CONTINUED ON PAGE SEVEN.)

## OUTFITS EN ROUTE.

### NEW SLAVE LAKE ROAD.

Since the issue of the last special,  
Dec. 16th, a new and shorter road to  
Peace river has been opened by the  
Northwest Government to Swan river,  
offering a bob-sleigh road from Ed-  
monton to Ft. St. John, and by the  
time the snow leaves the road will be  
opened to clear ground beyond Swan  
river, so that wagons can also make  
the through trip.

The distances are as follows:

	Miles.
Edmonton to Pembina river,	55
Pembina to Athabasca crossing,	42
Crossing of Swan river,	85
WINTER ROAD.	
Down Swan river to Slave lake,	25
Down lake to Slave lake post,	40
SUMMER ROAD.	
Swan river to Slave lake post,	50
S. L. post to Peace river crossing,	80
Crossing to Dunvegan,	85
Dunvegan to St. John,	115
St. John to Ft. Nelson,	200
	777

Up to and including the 16th of  
December last the following prospect-  
ing parties had left Edmonton for the  
gold fields by the following routes:

Mackenzie river,	130
Overland to Peace,	40
By water to Peace,	53
On snow to Peace,	223

Since that date the following parties  
have left on the overland route to the  
Peace, with flat sleighs:

Capt. Cadell, England; O. Wilson,  
Wales; L. A. Whitfield, Australia;  
Chas. Fisher, cook, and Geo. Rowland,  
of Edmonton, guide. They were met  
on Jan. 24th about 45 miles beyond  
Ft. Assiniboine.

R. W. Feltham, Chas. Conkling, J.  
H. Graves, C. Vogelzang, Springfield,  
Ill., and D. Muholland, Kingston,  
Cal., were met on the 27th of Jan.  
about six miles across the Paddle  
river.

Messrs. Hallwright, Simpson and  
Jeffries, of the Helman party, were  
seen at the Pembina river on the 27th  
of Jan. Mr. Jeffries was kicked by  
one of the horses and forced to return.

J. Hetu and Mallette Ducaux, of  
Montreal, and A. Chouinard, of  
Charlevoix, Quebec, were met on the  
22nd of Jan. about a mile across the  
Weyman river.

Messrs. L. Purdy and H. Thomas,  
of Johannesburg, and P. Oulette,  
of Edmonton, guide, left for Ft. St. John  
by way of the Landing and Athabasca  
river, and arrived at the Landing on  
4th Feb.

W. J. Morse, D. Hopkins, R. Dal-  
gleish, Ottawa; John A. Grose, Mon-  
treal; Dr. Brown, Stratford; Dr.  
Mason, John Orchard, S. Gooch, J.  
Bouret, Chicago, and T. Gregg and A.  
McLean left on 16th Dec. and were  
met on the Weyman river on the 22nd  
of Jan.

Geo. Pritchard, A. Nitchie, Chas.  
E. Hoskins, Frank Coles and T. F.  
Boond, of Newark, New Jersey, left  
about 20th of Jan. to travel up  
Athabasca river.

T. Heacock and Bryan, of North  
Dakota and Arthur Dando, of Edmon-  
ton, started for the Landing to go up  
the Athabasca on the ice on the 20th  
Jan.

O. D. Matheson, Wm. Allen and  
W. J. Magee, of Los Angeles, and G.  
W. Lang, of San Antonio, Texas, left  
for the Peace river via the Landing  
and up the Athabasca on the ice.

NOW OUTFITTING HERE.

The following parties are preparing  
to travel by the new Slave lake trail:

J. W. Durrand and J. Smith, Grand  
Forks, Dakota.

Daniel S. Low, Lynn, Mass.

Messrs. Norris, Derkes, Rued-  
ponsky, T. Henderson, C. Youngberg,  
H. Bagley, C. Barker, T. Hansen  
and C. J. Johnson, of Chicago.

J. P. Harling, W. M. Sherman, H.  
Freeman, Harry Miller, H. Groth,  
Frank Terrell, J. B. Howe, M. D., T.  
J. Hartigan and D. Burns, of Chicago.

M. T. Butler and R. G. Baldwin, of  
Los Angeles, and Murray Edgar and  
R. McFarlane, of New Brunswick.

W. H. Horner, Wm. Teisl, Ed. Bur-  
nett, John Bagge and J. J. Blair, M.  
D., Cincinnati.

A. and M. Monk, of Kingston, Ont.  
C. B. Heizer and wife, H. W.  
Woods and wife, Miss Sample, J. H.  
Myer, J. M. Heizer and Dr. Dom-  
hoff, from Cincinnati.

Dr. Fawcett, J. E. Smith, Jos.  
Durant, Alex. Johnson, Chas. John-  
son, Chas. and Nick. Bowen, Vic  
Chaffee, L. Matheson, Harry Handel,  
David Bliss, Thos. Neilson, Jas. Raf-  
ferty, John W. Fawcett, Pearl Ben-  
nett, Chas. Coloski, Jas. Ryder, Geo.  
Francis, John Gilmaker, W. W. Dick-  
son, R. Nicholson, Wm. Fiset, John  
Luck, David Beare, G. McGregor and  
Harry Peters. All the party are in  
camp about four miles out on the St.  
Albert trail.

Messrs. Thomas, Parker, Tallman  
and Weyrich, the "Sunny South"  
party.

A. Brookway, of Douglas, Wyo.,  
representing 40 or 50 mice who will  
follow.

Messrs. M. E. Putnam, B. Wit-  
tischen, Dr. Conley and C. H. Meyers.  
Three more to follow.

Messrs. Wm. F. Kaecke, Chas. E.  
Kruse, Adolph Hebert and Geo. Hees,  
of Chicago. In camp here.

S. H. Johnson and Chas. H. Schmidt,  
of Chicago.

The Montana party, composed of  
B. F. Wickwire, Geo. Weaver, Atkin-  
son and son, A. H. Taylor, W. Hill  
and Chas. Olson.

P. E. Curran, D. M. Parker, C. W.  
Waterman, J. A. Murphy, Jas. Car-  
roll, Fred. Deuch, L. Van Haxledem,  
S. W. Morgan, F. E. Atherton, W.  
M. Handelman and L. Parker, of  
Chicago. They expect to start this  
week.

F. G. C. Kelly, Richard G. Hall,  
John Milne and Alick Gill, of Dublin,  
Ire.

M. Patterson, Portage La Prairie.

Capt. Willis and a party of 35  
others arrived last night. The mem-  
bers of the party are: Capt. Allen,  
S. Willis, Morris L. Willard, Jos. M.  
Shenk, A. O. Brown, John Watson,  
Chas. B. Tapp, Geo. Enderly, Scotland  
Furnia, R. W. Campbell, Alf. J.  
Hurns, Chas. Thomas, Jacob John-  
ston, J. M. Swiggart, John Gregg,  
James Randall Day, T. S. Oliver, P.  
D. Hawley, Paul R. T. Kusch, Archi-  
bald Galbraith, Clarence Galbraith,  
Henry Lang, Thos. V. B. Cannon,  
Jos. H. Gaebel, M. C. Hirsch, Wm.  
Luther, Edward Kaffer, Chas. Cavan-  
agh, O. S. Carman, John S. Douglas,  
Wm. C. H. Duensing, Dr. J. A. Car-  
lstein, Messrs. Kleinman, Jos. Stoss,  
Principally of Chicago.

R. J. Battle, R. Van Atter, C.  
Einaudin, R. W. Smith, J. T. Mont-  
gomery, F. A. Davidson, F. Kenrick  
and Wm. Henry, of Toronto.

M. Prudhomme and D. McIntyre, of  
Ft. Steele.

E. G. Miller, W. Frey, D.



# MISCELLANEOUS.

**WANTED.**  
Cook wanted at Commercial Hotel, 80th Edmon-  
ton. Apply to  
**COMMERCIAL HOTEL.**

**WANTED.**  
A middle aged person as housekeeper for a family  
of five children. Can a farm 8 x miles from South  
Edmonton. Must be able to take full charge.  
Apply, stating wages expected, to  
**MRS. RICHARD DINWODIE,**  
South Edmonton, Alta.

## NOTICE.

The Edmonton, Peace River Transportation and  
Storage Co. is now in a position to do business.  
Your freight and goods transported carefully and  
quickly. Satisfaction guaranteed.  
**A. G. HARRISON,**  
Manager.  
Head Office, "Yukon Bureau of Information,"  
Edmonton, Alberta.

**ESTRAY.**  
Strayed away about the 15th July from my place  
at Spring Creek, one iron grey cow, branded B on  
left jaw, hole in left ear and a ear placed  
"H. B. Bibber, Manawan, N. B." Also one  
black two-year-old steer, branded and tagged the  
same. Information as to their whereabouts will be  
gladly received by A. J. Riley, Edna. 24 9

**FOR SALE.**  
One Karm piano for \$300 cash, and one small  
six stop Karm Organ.  
**MRS. CANN, Edmonton.**

**TWO RENT.**  
Mrs. Vorey's farm on Groat's Flat. Ten acres of  
land with house and stable. Rental \$50 per year.  
Apply to Mrs. John McEachern, Leduc. April 3

**ESTRAY.**  
Strayed from the pasture field of the undersigned  
one dark iron grey horse, three years old, branded  
on right shoulder. Finder will be suitably re-  
warded.  
**WM. HUMBERSTONE, Edmonton.**

**LOST.**  
Since spring, a red and white spotted cow, with  
all four horns broken off, branded A inside a  
horseshoe on left hip. Anyone returning the same  
to Philip C. Hamilton, Agrioon, will be suitably  
rewarded.  
**PHILIP C. HAMILTON,**  
Sec. 22, Tp. 53, R. 27.

**HORSES FOR KLONDIKE.**  
Two hundred head of northern horses, thorough-  
bred. At A. L. Brown's stable after January  
15th. Apply to J. H. Turnbull, Edmonton.

**ESTRAY.**  
A band of 45 or 50 horses, all branded B. Lost  
about a year from Egg Lake (Manawan). A reward  
of \$2.00 per head will be paid for every horse re-  
turned to  
**FRANK BIBBER,**  
Manawan.

**Furs Furs Furs**  
Jos. Ullman & Co., of St. Paul, have re-  
commenced for buying for the coming  
season at their old stand.

**THOS. HOUSTON,**  
Representative.

## NOTICE

It is hereby given that application will be made to  
the Parliament of Canada at its next session for an  
Act to incorporate a Company with power to con-  
struct, maintain and operate by any motive power a  
railway of standard or narrow gauge from a point in  
or near Edmonton, Northwest Territories, by the  
most feasible route to some point at or near the  
confluence of Smoky and Peace rivers or to some  
point between said confluence and Duvergne, and  
thence to any length, also to own, charter and  
run steamers and other vessels; to acquire, operate  
and dispose of mines, minerals and their products;  
timber lands, water powers and privileges; and to  
produce and utilize electricity for all purposes; to  
lease and operate telegraph, telephone, ferris, tele-  
graph and telephone lines, mills, wharves, docks and  
elevators; to do a general business as carriers; to  
do all and manage merchandise stores; to acquire the  
business of any other company or companies carry-  
ing on any similar business or businesses; and with  
all other powers useful, necessary or incidental to  
any of the objects above mentioned.  
Dated the 10th day of December, 1897.

**A. FERGUSON**  
Solicitor for the Applicants

## NOTICE

It is hereby given that application will be made to  
the Parliament of Canada at its next session, there-  
of for an Act to enable the Calgary and Edmonton  
Railway Company to extend its railway so as to  
make connection with the line of railway constructed  
or to be constructed, through the Crow's Nest Pass,  
and to issue bonds upon such extension; to pur-  
chase and acquire any railway or works already con-  
structed in view of or for the purpose of making such  
connection, and to issue securities for the same free  
from the lien of the existing bond debt of the com-  
pany; to change the head office of the company from  
Montreal to Toronto, and for other purposes.  
Dated at Toronto, December 6th, 1897.

**KINGSMILL, SAUNDERS & TORRANCE,**  
Solicitors for the Applicants.

**LAND TITLES ACT 1894.**  
**MORTGAGE SALE**

Pursuant to the order and direction of Honorable  
Mr. Justice Scott, dated the 20th day of January,  
1898, and under and by virtue of a certain registered  
mortgage, made under the provisions of the Land  
Titles Act, 1894, which will be produced at time of  
sale, there will be offered for sale by public auction,  
at Robertson's Hall, Edmonton, on Saturday,  
the 21st day of February, 1898, at 2 p. m., the following  
property, namely:

Lot 32 and 33, Block 5, River Lot 14, Edmonton.  
There is a suit on above property a comfortable  
dwelling.  
Terms and conditions of sale will be made known  
at time of sale, or upon application to  
**P. L. McNEIL,**  
Vendor's Advocate.

**W. S. ROBERTSON, Auctioneer.**

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sale, there will be offered for sale by public auction,  
at Robertson's Hall, Edmonton, on Saturday,  
the 21st day of February, 1898, at 2 p. m., the following  
property, namely:

The westerly halves of Lots numbers Seventeen  
(17) and Eighteen (18), Block 6, according to regis-  
tered plan "G" of record in the Land Titles Office  
of the North Alberta Land Registration District.  
The above property is situate at St. Albert, and  
upon which is a large frame store known as the  
Hill Store.  
Terms and conditions of sale will be made known  
at time of sale, or upon application to  
**BECK & EMERY,**  
Vendor's Advocates.

**Or to W. S. ROBERTSON,**  
Auctioneer.

**WHY take a Policy in the**

**New York Life**

**Insurance Company?**

**BECAUSE** you want the very  
best—the most privileges  
and the fewest restrictions  
—and the New York Life's  
Accumulation Policy fur-  
nishes it.

**G. M. DYER, Gen'l Agent.**

Office over Imperial Bank, Edmonton.

**Fresh Oysters**

Arriving on every train.

In bulk and served in every  
style

**At Clarke's Grocery**

# GOLDSEEKERS

## STOP AND CONSIDER.

1st—That the Farming District of  
Edmonton is surrounded by a Golden Belt  
stretching from Kootenay, Cariboo and  
Casuar to Klondyke, and from Klondyke to  
Kootenay.

2nd—That Edmonton is the Garden of  
these Gold Fields and is as fertile in agricul-  
tural resources as these mining regions are  
rich in minerals.

3rd—That the greatest beneficiary of  
fortune in these regions must be the Ed-  
monton farmer who feeds them all.

Illustrated description of the Edmonton  
District, containing experiences of over 50  
practical farmers, with accounts of its varied  
resources and climate, and also full  
information on every question of importance  
to intending settlers; to which is added a  
full description of the route from Edmonton  
to the Northern Gold Fields, mailed on  
receipt of 50 cents.

The Choicest Farms and Lands in the  
Edmonton District for sale at

## COWIE'S

Real Estate, Mining and Insurance Agency,  
BULLETIN BLOCK, EDMONTON.

**15 PER CENT.**

**Cash Discount Sale**

To make room for new goods arriving  
almost every train, I will for the next  
fourteen days give a discount of 15 per  
cent. off all cash purchases of \$1.00 and  
upwards.

For Camp Stoves, Mining Supplies and  
general hardware requirements this is a  
rare chance. Don't miss it.

This Sale to close Saturday, Feb. 21st.

No middlemen's commissions.

**JAS. A. STOVEL**  
Yukon Supply House.

## NOTICE!

Unless the arrears of taxes as given below on  
southwest quarter sec. 16, tp. 53, rge. 24, 4th mer-  
idian, are sooner paid the undersigned Treasurer of  
Turnip Lake School, No. 386, N. W. T., will proceed  
to sell the land for taxes and costs at the School  
House on Saturday, March 15th, at 10 o'clock a. m.  
sharp:

1894, \$8 00  
1895, 7 75  
1896, 7 20  
1897, 8 00  
1898, 2 75

Total, \$32 70

**E. DEAN,**  
Edmonton P. O.

"Should auld acquaintance be forgot."  
**EDMONTON OLD-TIMERS' ANNUAL.**

**Old-Time Ball**

MONDAY, FEBRUARY 21st, 1897.

Tickets \$2. Ladies free. Supper at the  
Jasper House.

H. S. Young, Pres. A. McNeil, Sec.  
J. Gibbons, E. Looby, Floor Managers.

Tickets for sale with Floor Managers and  
Officers of the Association.

**LAND TITLES ACT, 1894.**  
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the 21st day of February, 1898, at 2 p. m., the following  
property, namely:

Lot 32 and 33, Block 5, River Lot 14, Edmonton.  
There is a suit on above property a comfortable  
dwelling.  
Terms and conditions of sale will be made known  
at time of sale, or upon application to  
**P. L. McNEIL,**  
Vendor's Advocate.

**W. S. ROBERTSON, Auctioneer.**

## NOTICE

It is hereby given that application will be made to  
the Parliament of Canada at its next session, there-  
of for an Act to enable the Calgary and Edmonton  
Railway Company to extend its railway so as to  
make connection with the line of railway constructed  
or to be constructed, through the Crow's Nest Pass,  
and to issue bonds upon such extension; to pur-  
chase and acquire any railway or works already con-  
structed in view of or for the purpose of making such  
connection, and to issue securities for the same free  
from the lien of the existing bond debt of the com-  
pany; to change the head office of the company from  
Montreal to Toronto, and for other purposes.  
Dated at Toronto, December 6th, 1897.

**KINGSMILL, SAUNDERS & TORRANCE,**  
Solicitors for the Applicants.

**LAND TITLES ACT 1894.**  
**MORTGAGE SALE**

Pursuant to the order and direction of Honorable  
Mr. Justice Scott, bearing date the 20th day of January,  
1898, and under and by virtue of a certain registered  
mortgage, made under the provisions of the Land  
Titles Act, 1894, which will be produced at time of  
sale, there will be offered for sale by public auction,  
at Robertson's Hall, Edmonton, on Saturday,  
the 21st day of February, 1898, at 2 p. m., the following  
property, namely:

The westerly halves of Lots numbers Seventeen  
(17) and Eighteen (18), Block 6, according to regis-  
tered plan "G" of record in the Land Titles Office  
of the North Alberta Land Registration District.  
The above property is situate at St. Albert, and  
upon which is a large frame store known as the  
Hill Store.  
Terms and conditions of sale will be made known  
at time of sale, or upon application to  
**BECK & EMERY,**  
Vendor's Advocates.

**Or to W. S. ROBERTSON,**  
Auctioneer.

**WHY take a Policy in the**

**New York Life**

**Insurance Company?**

**BECAUSE** you want the very  
best—the most privileges  
and the fewest restrictions  
—and the New York Life's  
Accumulation Policy fur-  
nishes it.

**G. M. DYER, Gen'l Agent.**

Office over Imperial Bank, Edmonton.

**Fresh Oysters**

Arriving on every train.

In bulk and served in every  
style

**At Clarke's Grocery**

# TO THE YUKON.

(CONTINUED FROM PAGE SIX.)

Axe and handle,	5	\$1 25
Hatchet,	1	50
Set files,	1	50
Nails,	10	80
Sundries:		
Scythe and fittings,	7	1 50
6 hanks snaring wire,	1	16
6 traps, assorted,	6	1 50
Gilling twine,	2	2 00
1 bailer,	1	50
2 set gold scales,		3 00
Fish hooks, chemois, goggles, etc., awl, gimlet,		
scissors, etc., sundries,	3	5 24
	50	\$25 00

Double outfit, half to be charged to each man:

Set grizzly irons,	16	60
Iron barrow wheel,	11	1 25
Whip saw,	10	7 00
Hammer,	1	50
Hand saw,	2	1 25
Augur, 1 1/2 inch,	1 1/2	85
Brace and bits,	4	1 00
Chisel and cold chisel,	1	65
Large kettle,	4 1/2	1 50
Folding reflector,	4	2 00
Tracking line,	12	3 00
Pitch,	10	80
Oakum,	15	1 20
Wedge tent, 7x9, 8 oz. duck,	15	9 00
Fibre tub,	3	1 00
Caulking iron and sundries,		3 40
	110	\$35 00

Sundries and clothing, special:

2 pair H. B. blankets, 4 pt. @ \$9 50,		\$19 00
(3 pt. blankets per pair, \$6)		
1 yard duffel for heavy socks at \$1 40,		1 40
4 yards stroud for grizzly blankets, at \$1 40,		5 60
2 pair beef moccasins, fur lined, per pair, \$1 50,		3 00
Rubber boots, hip,		5 00
2 suits heavy underwear, at \$2 25,		4 50
6 pair wool socks,		1 50
4 pair overalls at \$1 25,		5 00
Leather jacket, sheep lined,		10 00
Mackinaw shirt,		3 50
2 pair wool mitts,		1 00
2 pair leather mitts,		1 00
Slicker,		4 00
Rubber sheet, 4 feet by 6,		1 50
2 dunnage bags,		2 00
Needles, thread, etc., and sundries,		7 00
		\$75 00

Weight, say 75 pounds.

This brings the cost of outfit to each man up to \$195 and the weight to  
1,250 pounds and does not include a gun or ammunition. The most useful  
gun for the miner, as distinguished from the hunter, to take is a double-barrelled  
muzzle loading shot gun, which will cost up to \$15. There is less danger of  
breakage in rough handling than in case of a breechloader and also less danger  
from running out of ammunition. A shotgun is vastly more useful than a rifle  
for killing game and a rifle is not needed for self defence. Add cost and weight  
of gun and ammunition and the outfit will cost slightly over \$200 with a  
weight of 1,300 pounds per man for one year. A good breechloading shot gun  
will cost \$25 and cartridges \$3 per 100. A Winchester rifle 44-40 will cost  
\$17 and cartridges \$1.70 per 100.

## PACK TRAIN.

Two hundred pounds is a maximum load for a pack horse on a long road.  
Where the road is new and the packers new at the business the loads should be  
still lighter, and 170 pounds should be the maximum. The mistake made by  
the men taking the pack trail this season was that they loaded too heavily. A  
few pounds of overload on a pack horse tells more heavily on both horse and  
man than on any other kind of conveyance. Eight horses to pack and one to  
ride and one spare is the full number for each man's outfit if a year's supply is  
to be taken and good time made.

Ten horses @ (\$25 to) \$35,	\$350 00
Riding saddle and bridle,	15 00
Eight pack saddles, @ \$5,	40 00
Eight pack covers, 5x6, 12 oz. duck, @ \$1.50,	12 00
Sixteen saddle bags, 12 oz. duck, @ 45c.,	7 50
Ten set hobbles,	5 00
Five halters,	5 00
Ropes and sundries,	5 50
	\$440 00

**TOTAL MAXIMUM COST, LAND ROUTE.**

The account then will stand:

Supplies,	\$200 00
Extras for trip to Nelson river,	50 00
Pack outfit of ten horses,	440 00
Unforseen,	50 00
	\$700 00

In counting this cost it should be remembered that the pack outfit is  
worth its cost for future use, so that this outlay is not properly chargeable to  
one year's expense. The prices quoted for horses are higher than has been  
generally paid hitherto, but in horses or anything else in this connection the  
best is the cheapest. The prices of supplies are also the ordinary retail figures  
for single articles of best quality and will be bettered very considerably where  
an outfit is being purchased. Horses have been purchased by parties here at  
from \$5 to \$15, but experience has shown that they are the dearest in the end,  
for they cannot carry the loads nor make the time required. This price, how-  
ever, has been somewhat raised by the increased demand, but it is expected  
that large bands will be brought in from Southern Alberta and Montana in the  
early spring, when the price will probably drop again.

It is generally considered that six loaded horses are as many as one man  
can handle satisfactorily. This being the case it would be well for two men  
going together to purchase two more horses and supplies to load them and hire  
an extra man to go to Pelly river, (after which he would not be required) rather  
than load 12 horses with 200 pounds apiece.

**C. S. LOTT, CALGARY,**  
District Appraiser for Alberta.  
For application forms and information, call on  
**G. T. BRAGG, EDMONTON.**

**13-30**

**D. R. FRASER'S**

**LUMBER & FLOUR**

**. Mills .**

**FLOUR AT A GREAT**

**REDUCTION.**

A large and assorted stock  
of Dry Lumber on hand which  
has been lately augmented by  
the purchase of Fraser & Mc-  
Kernan's stock.

British Columbia Fir and  
Cedar Lumber, Shingles, Lath,  
Mouldings and Casings, Doors  
and Windows.

# The Bulletin...

During the next six months will make a specialty of securing  
and publishing information regarding the Yukon and the way  
to get there. Reports will be received from exploring parties  
and prospectors, and information of Gold strikes will be re-  
ported at once. Papers mailed to any part of the United  
States or Canada without charge for postage. Remit sub-  
scriptions by Post Office Order. Paper published twice a week.

**Subscription, Six Months for \$1.00**

**Better than Dawson City!**

**MORE MEAT**

**AT VANCE'S**

**than the Klondyke.**

Struck it rich in Beef, Pork,  
Mutton, Chickens, Geese,  
and Turkeys

And all mixed up together in  
Sausages.

Terms as usual—to customers, cash in  
thirty days.

**- The Best is the Cheapest -**  
**MINERS AND PROSPECTORS**  
Preparing for the Great  
Northern Eldorado, require

# .OUTFITTING.

The cheap goods will weigh as much as the best  
grades, so it pays you to buy the best.

We are now prepared to fill all Outfitting Orders  
for Hardware—and quote only on the best  
grades. Call and examine our

**Metal Fire Box Camp Cook Stoves**

List of Hardware requirements furnished on  
application.

**J. L. JOHNSON & Co.**

Hardware Merchants, Edmonton, Alberta.

**..ENGLISH GOODS..**

I have pleasure in directing attention to the Enormous  
Consignment of British Goods just opened at the Man-  
chester House. Having purchased for cash on specially  
advantageous terms, I have secured some great bargains,  
in which all purchasers at the Manchester House will  
participate.

Make a point of securing some of the Lovely Dress  
Materials so suitable for evening wear, at from 25c. per yard  
and upwards.

Special attention is directed to some unusual bargains  
in Ladies' Flannelette Underwear, Hosiery, Scotch Wools  
and Shawls.

Long Silk Gloves (36 inches) all shades.

Terms Strictly Cash. Positively no exceptions.

**W. JOHNSTONE WALKER,**

**...THE MANCHESTER HOUSE.**

**F. FRASER TIMS**

**GENERAL AGENT**

**FOR THE ONLY**

**All-Canadian**

**Route**

**...TO THE GOLD FIELDS...**



# THE MACKENZIE ROUTE.

While the overland route from Edmonton offers special advantages to the prospector desiring to reach the upper waters of the Yukon, and to get in with horses so that he can move about freely when he gets there, or to persons desiring to drive in beef cattle for their own use or for sale, the water route by way of the Mackenzie offers advantages vastly superior to any other route, to the miner whose objective point is the established workings at Dawson City, Forty-mile, or Circle City, and who wishes to reach there with one or two years' provisions. Very few of those who have joined in the rush of this season have actually reckoned the amount of provisions and other supplies which they will require for a year and consequently hundreds who have gone believing they had a year's supply will find themselves short in six months. A healthy hardworking man requires from two and a half to three pounds of food every day of his life or say 1,000 pounds for a year, 1,500 pounds for 18 months and 2,000 pounds, or one ton, for two years. He will require, besides, implements and clothing to the amount of two or three hundred pounds. A man will take from two to three months getting into the Yukon by any of the routes. If he starts in March with only 1,000 pounds of food he will only get into the country in June. The winter closes in in September. He has only had three months of prospecting, and if he does not get out before the next March he will be out of food. If he can get in with 1,500 pounds, or 18 months' supply, he will have two summers prospecting in sight instead of only one; and if he strikes anything the second summer he can remain and work all winter if he has a two years' supply with him. No matter how much supplies he has he will have use for all if he stays in the country, and if he does not stay what he has to spare will always be worth gold. By the Mackenzie route the miner can reach the Yukon with a two years' supply of provisions for very little more than his bare passage from the coast to Dawson will cost him, and for a fraction of the money that it will cost him by the Chilkoot or Stickeen routes. The rivers furnish the motive power for almost the whole of the distance and the time taken is little if any longer than by the mouth of the Yukon. The open season is practically much longer than by that route, the reason being that the journey down the Mackenzie can be begun long before the ice has cleared out of Behring sea, and Fort Yukon can be reached under favorable conditions by way of the Mackenzie before the long voyage up the Yukon can be begun. The open season by way of the Mackenzie is long if the forepart is taken advantage of, as water is open at the southern starting point long before it is at the portage from the Peel to the Porcupine; and the Mackenzie is by far the longest part of the route. On the other hand if a late start is made from the southern end it is very short, too short indeed to be gone through in the one season. The difficulty with the large number of parties who have started over this route this season is that they all started too late to have a reasonable chance of getting through this year. However, they all had the advantage that they need not limit themselves in the quantity of supplies taken and therefore need not be at any serious loss by reason of the supplies consumed this winter.

## PROSPECTS ON THE WAY.

In the vast stretch of country lying between Edmonton and the Yukon, there are many indications of various kinds of minerals. Although it has been scarcely prospected as yet, enough is known to warrant the careful attention of the prospector and the capitalist for the whole distance.

At Athabasca Landing natural gas was struck by a government party who were boring for oil.

At Pelican Rapids, 120 miles below the Landing, a government party struck a heavy flow of gas and afterwards oil at about 800 feet. The flow of gas was so strong that they were compelled to abandon the well for the season. From the Pelican Rapids northward for 200 miles along the Athabasca are unmistakable indications of petroleum, the tar oozing out of the banks for the greater part of that distance.

At Fort Chipewyan on Lake Athabasca, 528 miles from Edmonton, the western extension of the Laurentian formation in which the Rainy River and Lake of the Woods gold mines are situated, is reached. The north shore of Lake Athabasca is entirely in this formation and offers good inducements for prospectors, as it is easily reached. From Chipewyan northward the waters of the Mackenzie form the westerly boundary of this Laurentian formation. At its mouth the Mackenzie constitutes the dividing line between the gold bearing Laurentian range of the east and the gold range of the Pacific coast, which extends practically unbroken from the United States boundary to the Arctic ocean.

A short distance below Fort Smith on Salt river, which puts into Great Slave river from the west, are vast deposits of pure salt from salt springs. These salt deposits supply all the salt used in the Mackenzie region, no preparation being required.

On Great Slave lake, near its western extremity, are petroleum tar, and sulphur springs.

The eastern and northern arms of Great Slave lake extend into the Laurentian formation and samples of quartz brought last summer from the shores and islands of this lake were rich in gold. This lake is nearly as large as Lake Huron, and is so easily accessible for machinery and supplies as to offer special advantages for the development of its numerous quartz reefs.

The Liard river, which is the second great tributary of the Mackenzie on the west, heads in several branches in the gold range which lies west of the Rockies. The Dease, upon which the celebrated Cassiar diggings are situated, is its most southerly branch. The main or middle stream has never been fully prospected in its upper part, but is reported to be very rich in both quartz and placer; the northern branch called the Frances, which interlocks with the Pelly, the main stream of the Yukon, to which it gives access by the overland route, is also reported by Prof. Dawson to be rich in placer and giving excellent indications in quartz. The Liard river was the route by which the Hudson's Bay Company first pushed trade into the Yukon and established Fort Selkirk at the junction of the Pelly and the Lewes. As already stated this route can be taken in boats from Fort Simpson, but it is a very long distance up stream, and about 40 miles of the river above the mouth of the Nelson is very difficult. There are also two hard portages, one of four miles and one of two miles, besides five short ones, before reaching the Frances. However, determined men who are good boat men can make it, with suitable boats carrying about 1,200 pounds to the man.

North of Fort Simpson at the mouth of the Liard, the Mackenzie receives a number of minor tributaries from the west, which unquestionably head near if they do not interlock with the sources of the Pelly, the Macmillan and the Stewart. If it be true that the gold comes from the heads of the streams, the head of the gold in the Yukon is nearer the main stream of the Mackenzie than the main stream of the Yukon, and possibly can be reached more easily up these minor tributaries of the Mackenzie. It is well known that Indians travel from the head waters of the Yukon to the Hudson's Bay posts on the Mackenzie, but no definite information is available as to the nature of the routes beyond those by way of the Liard and the Peel.

Peel river is the third large westerly tributary of the Mackenzie, the others being the Peace and Liard. It is half a mile wide at Fort Macpherson, 16 miles above its junction with the Mackenzie, and beyond about 60 miles above Fort Macpherson it is absolutely unexplored. Having such an immense volume of water it must drain a large area and interlock with the sources of the Yukon, as does the Liard. Ogilvie reports that when travelling from the head of a small tributary of the Yukon called the Tat-on-due to the head of the Porcupine, he crossed the head waters of a branch of the Peel river. This was within 30 or 40 miles of the main stream of the Yukon and not more than 100 miles from Dawson City. This being the case there is every reason to expect to find as good bar mining on the upper branches of the Peel as on the Yukon, and that the Peel offers as direct a route to the rich placers of the central Yukon region as does the Yukon itself. The difference in the cost of supplies laid down on the Peel by the Mackenzie down stream route and on the Klondyke by the Yukon up stream route need only be mentioned to be understood.

In his recent lecture on the Yukon in Victoria, Ogilvie mentions that Harper and McQuestion, the first English traders to establish themselves in the Yukon besides the Hudson's Bay Company, and who built Fort Reliance near the site of Dawson City, went into the Yukon from the Omenica mines on the Peace river by the Mackenzie river route. They were miners, and prospected on the way. They told Mr. Ogilvie that on the Peel they found fair prospects, but as they were on their way to the Yukon they did not delay to prospect extensively on that river, and indeed only saw it near its junction with the Mackenzie.

The Porcupine, or north branch of the Yukon, which forms part of the Mackenzie river, has never been prospected seriously. Harper and McQuestion found colors on it. This river as well as the Peel, rises within 100 miles of Dawson. After a northeasterly course which brings it within 100 miles of the Mackenzie it turns sharply southward. Rising as it does within a comparatively short distance of Forty-mile and Klondyke there is every proba-

bility that in its upper part, if not in its lower, it would pay for the attention of the prospector.

Taken altogether, there is no like area in the known world so easily accessible which offers equal possibilities to the prospector or the investor as the country along the Mackenzie route to the Yukon.

It is worthy of mention that United States whaling vessels operate in the Arctic ocean, and winter at Herschell island, 80 miles west of the mouth of the Mackenzie. Several travellers have taken passage on these vessels out to civilization.

East of the mouth of the Mackenzie the Coppermine river, which rises in the Laurentian range east of the Mackenzie, empties into the Arctic ocean. This river is named from the amount of pure copper which is found on its banks. Lead is also found in the same locality, and it is quite possible that gold exists in conjunction with the copper or the lead, or perhaps with both. While it is possible that the copper or gold would not pay to work alone, it is also possible that gold might exist in conjunction in paying quantities.

One hundred and twenty persons left Edmonton for the Yukon last season by the Mackenzie route.

## BOATS.

By the Mackenzie route the boat is the means of conveyance and the size and build of the boat should be regulated by the amount of supplies which it is desired to take and the number of men accompanying. A boat is required large enough to carry all the supplies that are needed and yet not so large that it cannot be handled easily in the portages by the men available. It will have to drift down stream, shoot rapids, sail on lakes Athabasca and Great Slave, be tracked in shallow water up Poplar or Trout river to McDougall pass, will have to be portaged in several places, and finally be tracked or sailed up the Yukon. There is plenty of room for argument as to the boat that should be used under all these circumstances, but after most careful enquiry the BULLETIN has arrived at the following conclusion, provided a party of four to eight men are going, desiring to take from 1,500 to 2,500 pounds of supplies each: The build of boat best adapted to all the circumstances to be met with on the route is the well known Hudson's Bay Co. York or inland boat. A boat of this build, 24 feet keel, 8 feet beam and 2 feet 9 inches depth is known as a half sized York boat. It will carry in all weathers and circumstances from 2½ to 3 tons. Four men would be a full crew. Such a boat would weigh about 1,200 pounds and would cost at Edmonton \$150. At the Landing the cost would be about \$175. Sailing tackle extra. This boat could be managed easily in good water down stream or under sail by two men. If two such boats accompanied each other the crews could be doubled in passing rapids, and on portages eight men would be required. Four such boats with eight men, not afraid to work, and starting from the Landing with the first open water in April or even on the high water of the latter end of May, could reach the Yukon early in August, with two and a half years supplies to the man. The total amount of portaging of loads in that distance would be

Grand Rapids,	1½
Cascade Rapids (possibly),	1
Fort Smith Rapids, 4 portages ½ mile each,	1
McDougall Pass,	1
Total,	18½

Portage of boats:	Miles.
Three portages in Smith Rapids, ½ mile each,	3
McDougall Pass,	1
Total,	4

All these portages except one of those at Fort Smith are over level ground, as compared with the 35 miles with 3,500 feet climb over the Chilkoot pass. Total distance of up stream navigation from Athabasca Landing to Fort Yukon:

Slack water on Trout river,	15
Swift water on Trout river,	24
Tributary of Trout river to Summit lake,	7
Total,	46

Boats of other build costing less according to carrying capacity than the York boat may be had, but it is poor economy to start out on such a long trip, where everything depends on the adaptability and manageability of the boat with anything but the best, particularly as there must, for many years, if not for all time, be a great deal of the prospecting and carriage of supplies to the upper waters of the Yukon done by boat. Therefore, a good boat will necessarily be worth its cost on reaching there, while a poor boat is a danger and delay on the road and a loss on arrival.

Boats are built of any size and style either at Edmonton or at the Landing, to order. The boats built for use by Yukon bound prospectors this season were flat boats, sturgeon heads and clinker built skiffs. They were ordered in preference to the York boats because they could be put together more cheaply and quickly, costing only from a half to a quarter as much as a York boat, but they were poor economy. Parties starting in the spring should send one of their number ahead to order the boat, giving from a week to two weeks for construction if a York boat or sturgeon head is ordered. In that case there would be no delay on the arrival of the main party.

Boats built at Edmonton can be freighted to the Landing—if not too large—at a cost of three-quarters to one cent a pound. During the past season lumber could only be had at the Landing if whipsawed there or freighted out from Edmonton. A saw mill is being established there during the present winter by Alex. Fraser and an unlimited quantity of lumber sawn specially for boats will be available at the Landing next spring at from \$20 a thousand feet up, according to dimensions, for rough lumber; dressing, \$2.50 to \$3.00 per thousand extra. There will, therefore, be no delay for want of boat lumber this season nor any reason for complaint as to its price. Two boat building establishments have been started at the Landing this winter besides those at Edmonton.

## ATHABASCA LANDING.

From Edmonton, the terminus of the railway, to Athabasca Landing, the beginning of navigation on the Mackenzie route is 96 miles. There is a good sleigh or wagon road. The trip occupies from two to four or five days according to the weight of the loads and the state of the roads, and the rate of freight is one cent a pound in winter or three-quarters of a cent in summer. There are no hotels north of Edmonton and only one or two stopping places, so that the traveller must be prepared to camp out on his way to the Landing, and at the Landing as well.

## ATHABASCA RIVER.

The Athabasca at the Landing is about 200 yards wide and opens from the 10th to the 25th of April. A guide is not absolutely necessary for the first 150 miles from the Landing, but below that point from the head of Grand Rapids to Ft. McMurray one is absolutely necessary. As most of the guides live about 50 miles west of the Landing at Lac la Biche, they are generally engaged at the Landing for the trip to McMurray. They are paid \$50 and board for the trip. Boatmen to help over the portage at Grand Rapids and through the rapids below may be engaged at \$25 each for the trip. But if the boats are fully manned or if the crews of several boats will double through the rapids extra men are not necessary.

There is no impediment to navigation until Pelican rapids is reached, 120 miles below the Landing. These rapids are not dangerous, but of course care is needed. There is no danger in high water, but in low water rocks are to be guarded against.

Grand rapids is reached 45 miles below Pelican rapids. The boat is landed where the steamboat also lands on the right bank of the river a mile and a half above the head of the rapids. Half the load is taken out and the boat taken to the head of an island which lies right in the rapids. A return trip has to be made for the second part of the cargo. The cargo is then carried to the lower end of the island which is also the end of the rapids, half a mile. The empty boat is let down alongside the island by a line and loaded at the foot of the island. The landing at the foot of the island is difficult. Two miles of rough water follow below the island, which require care in navigating in low water. The H. B. Co's steamer Athabasca runs between Athabasca Landing and Grand Rapids. The run by row boat from the Landing to Grand Rapids can be made in two and a half days.

Twenty-five miles below Grand rapids is the Bruie rapid, which is run near the left hand shore. Sixteen miles below is the Boiler rapid, which in high water is run on the left hand side and in low water on the right. In the 15 miles below the Boiler rapid occur the Drowned rapids, run on the

left hand side; the Middle rapids, run on the left; the Long rapids, run on the right; the Crooked rapids, run on the right; the Stony rapids, run on the right; the Little Cascade and the Big Cascade rapids. The channel in the latter is on the left, and can be run at high water; but in low water there is an abrupt fall of three feet, and a part or the whole of the load is portaged a few hundred yards and the lightened boat let over on a line. The Mountain rapid—the last of the series—is 10 miles below the Cascade. It is entered on the left side. In the middle the river is crossed to the right, the lower part being run on that side. A riffle five miles further down is called Moberly rapid, and is passed on the left hand side. In a good stage of water boats from Grand Rapids to McMurray make it in one and a half to two and a half days. These numerous rapids should not be attempted without a guide, and a full crew should be in each boat. As they are all short, boat's crews can be doubled, if short of their full number, in going through each one, without much loss of time. Total distance, Grand Rapids to McMurray, 87 miles.

## LAKE ATHABASCA.

McMurray to Lake Athabasca, 170 miles; good navigation, no impediments.

Lake Athabasca to Chipewyan, 10 miles across lake. Shore must be followed westward from mouth of Athabasca to opposite peninsula on which Fort Chipewyan is situated.

## GREAT SLAVE RIVER.

Chipewyan to Rocky river, the outlet of Lake Athabasca, 10 miles north, westerly along shore and amongst islands. Local guide an advantage for this. Rocky river to junction with Peace, 30 miles; good navigation.

Peace river junction to Smith Landing, 75 miles; good navigation. Smith Landing to Ft. Smith, 16 miles; land portage for that distance if preferred. Rate of freight, 50c. per 100. Guide necessary if river is followed; can be procured in adjoining Halfbreed settlement. Guide charges \$25, and extra men to help over portages, \$15 each for trip.

The rapids are: Casettes Rapid—Boats and loads are portaged ¼ mile, level ground. Next rapid is generally run without portage.

Mountain Rapid—Boats and loads portaged ¼ mile over hill 100 feet high. Block and tackle used to get boat up.

Pelican Rapid—Boat and load portaged ¼ mile; level ground.

Drowned Rapid—Boat is generally run down with half load. With full crews about two days are generally occupied on these portages.

H. B. Co. steamer Graham runs between Smith Landing and McMurray. The Roman Catholic mission also has a steamer on the same stretch of navigation.

## GREAT SLAVE LAKE.

Ft. Smith to Ft. Resolution on south shore Great Slave lake, 196 miles; good navigation.

Ft. Resolution, at entrance, to Ft. Providence, at outlet of Great Slave lake, westward along south shore of lake, 168 miles. Difficult and dangerous for flat boats except with fair breeze, but all right for York boats. Slave lake opens from June 15th to July 10th.

## MACKENZIE RIVER.

Providence down the Mackenzie to Simpson at mouth of Liard, 161 miles; good navigation.

[An alternate route may be taken to Pelly river up the Liard, from Simpson to junction of Nelson river, a point on the overland route, 240 miles, tracking up stream. Navigation fairly good. If this route were taken a full crew of four to five men would be needed to each half sized boat.]

Simpson to Wrigley, 136 miles; Norman, 184 miles; (river opens at Norman from May 12th to May 28th and closes Oct. 7th to Nov. 2nd) Good Hope, 174 miles; mouth of Red river, on west bank, 214 miles; delta of Mackenzie, 48 miles; Peel river, taking first westerly channel of delta, 32 miles.

Ft. Macpherson, 13 miles up Peel, is the commencement of the 60 mile land portage to Bell branch of Porcupine, used in winter by the Hudson's Bay Co. Peel river is half a mile wide at Macpherson and is said to close from 10th to 20th September. The Peel comes from the gold bearing region of the Yukon and has never been prospected.

The H. B. Co. propeller Wrigley runs between Ft. Smith and Ft. Macpherson, there being uninterrupted navigation. The Roman Catholic mission also have a steamer on that route. But the H. B. Co. and mission steamers only ply for their owners, not taking general traffic.

## PEEL RIVER PORTAGE.

The boat route crosses Peel river at the delta channel by which it is entered and goes up Trout river, which enters almost immediately opposite from the west, 15 miles, in slack water.

Then follows 24 miles of the same stream against a stiff current, but without portages. Then a small tributary of Trout river from the west 7 miles. Then a portage of 300 yards across the summit of McDougall pass a creek flowing westward into the Bell river branch of the Porcupine which is the north branch of the Yukon. Down this stream six miles to Bell river.

From Trout river to Bell river is 14½ miles, in a level, grassy and nearly treeless valley. This is the McDougall pass through the Rocky mountains. Jas. McDougall went through this pass, both ways, in the season of '72 with a boat and crew of five men, taking 1,000 pounds of trading goods besides their own supplies. W. Ogilvie, D. L. S., crossed with loaded canoes in 1884, and surveyed it, and R. G. McConnell, of the geological survey, sent his boat through from east to west in '91. The summit of the pass is 1,100 feet above the waters of the Mackenzie. A guide across this pass should be secured at Ft. Macpherson.

## PORCUPINE RIVER.

McDougall pass to the Porcupine, 50 miles; good navigation down Bell river.

Down Porcupine river to Ft. Yukon, 275 miles; excellent navigation.

Ft. Yukon to Circle City, 90 miles; to Forty-mile creek, 100 miles further, and to Dawson, 60 miles further up the Yukon.

To the new diggings at Little Minook, 150 miles down the river from Ft. Yukon.

Total distance, Edmonton to Dawson, 2,500 miles.

## COMPARATIVE COST OF TRIP.

The following is a statement of the probable cost per man of a trip to the Yukon by the Mackenzie route with two years supplies, weighing 2,500 pounds, two men to a boat:

Half cost of boat,	\$ 75 00
Half cost of guides and help,	5 00
Food, clothing and hardware for first year as per list of prices elsewhere,	200 00
Provisions for second year,	80 00
Sundries for second year,	20 00

Total cost of two years supplies, weighing 2,500 pounds, landed at Fort Yukon by three months work, \$425 00

Cost of passage from Victoria to Dawson by river steamer \$150, without a pound of supplies.

Cost of packing 2,500 pounds of supplies over Chilkoot and White passes, exclusive of passage, freight, cost of boat, etc., at 20c. a pound, the lowest rate paid last season, \$500. At 50c. a pound, which was the standard rate of last season and will likely be the rate of the coming season, \$1,250.

## Two Cottages and Four Main St. Lots . . . FOR SALE . . .

(1) One or both of the cottages opposite the residence of D. W. Macdonald on McDougall Street.  
(2) Lots 19 and 20 on Jasper Avenue in River Lot 6, commencing 50ft. west of the Massey-Harris warehouse; will be sold in lots from 25ft. up to 100ft. as desired by purchaser. For terms apply to

H. C. TAYLOR, Barrister,  
OWNERS AGENT.